

THE  
RACING RULES  
OF  
OFFSHORE  
POWERBOATING  
EXTREME CAT  
X-CAT SERIES  
2011/2012

Ratified by UIM Council on 22<sup>nd</sup> October 2011  
for an implementation as from 1<sup>st</sup> November 2011

*“We give credit to WPPA on the work done since 4 years to elaborate and make the racing rules for X-CAT simple and objective”*

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## INTRODUCTION

The Racing Rules of X-Cat includes three main sections. The first “Parts 1–3”, contains rules that affect all competitors. The second “Part 4” provides details of rules, rules that apply to Class Promoter racing which affect only competitors or officials. The third section “Appendices 1–6”, provides details of rules and Guidelines required for the events.

No expressed or implied warranty of safety shall result from publications of, or compliance with these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to participants, spectators, or others.

The racing rules are revised and published every year by the (UIM), the world governing body for the sport.

This edition becomes effective November 1, 2011 and supersedes all previous editions. Any changes determined to be urgent during the season will be announced through national authorities and X-CAT Class Promoter and posted on the UIM website. [www.uimpowerboating.com](http://www.uimpowerboating.com)

# PART 1

## FUNDAMENTAL RULES

01. FAIR RACING
02. ACCEPTANCE OF THE RULES
03. DECISION TO RACE
04. BANNED SUBSTANCES AND METHODS

### 05. MISCONDUCT

#### 01. FAIR RACING

A driver and team owner shall compete in compliance with recognised principles of sportsmanship and fair play. A driver may be penalised under this rule only if it is clearly established that these principles have been violated. A disqualification under this rule shall not be excluded from the boat's series score.

#### 02. ACCEPTANCE OF THE RULES

By participating in a race conducted under these racing rules, each driver and team owner agrees

- (a) to be governed by the rules;
- (b) to accept the penalties imposed and other action taken under the rules, subject to the appeal and review procedures provided there in, as the final determination of any matter arising under the rules; and
- (c) with respect to such determination, not to resort to any court or other tribunal not provided in the rules.

#### 03. DECISION TO RACE

The responsibility for a driver's decision to participate in a race or to continue racing is his alone.

#### 04. BANNED SUBSTANCES AND METHODS

A driver shall neither take a substance nor use a method banned by the Olympic Movement Anti-Doping Code or the World Anti-Doping Agency, unless a T.U.E. has been granted.

#### 05. MISCONDUCT

Rules and Regulations exist for the safety and security of competitors, officials, and the public and for the orderly conduct of racing events and must be followed. All team members are required to behave in a responsible and acceptable manner throughout the duration of any event. Any behavior which acts to disrupt the smooth running of the events the series or could be deemed to have brought the sport in to disrepute may be subject to penalties (which may include disqualification, suspension from an event, reprimands, yellow cards, expulsion or financial penalties may be applied.

Abuse of Officials – Instances of verbal or physical abuse against any race or event Official or competitor will result in a penalty. Penalties may include suspension or disqualification from an event, exclusion from the Series/ Championship or imposition of a points and/or fines.

# PART 2

## GENERAL APPLICATION

### SECTION A

01. GENERAL APPLICATION TO ALL EVENTS
02. GENERAL PRE-RACE REQUIREMENTS FOR CONDUCT OF EVENTS
03. COMPLIANCE WITH RULES AND REGULATIONS
04. INTERPRETATION OF THE RULES
05. AWARDS AND PRIZE MONEY
06. PAYMENT OF FINES
07. CONSUMPTION OF ALCOHOL
08. ANTI-DOPING REGULATIONS

### SECTION B

09. INDIVIDUAL REQUIREMENTS FOR PARTICIPATION IN AN EVENT

### SECTION C

10. UIM RULE CHANGE SUBMISSIONS PROCEDURE

### SECTION A

01. GENERAL APPLICATION TO ALL EVENTS

Unless otherwise specified the following General Racing Rules apply to all UIM Races/events, and Special Events and time/speed trials.

A Race is defined as from start of Registration time on Day 1 to end of Final Day closure of Race Administration, unless stated otherwise within the UIM Rules.

All stated numbers of days for any post race correspondence is considered to be in accordance with the International dateline and not the race/event local time.

02. GENERAL PRE-RACE REQUIREMENTS FOR CONDUCT OF EVENTS

Event Participation - Teams are encouraged to participate in all media opportunities, autograph sessions, sponsor parties, parades, etc.

03. COMPLIANCE WITH UIM RULES AND REGULATIONS

Race Teams must comply with all UIM rules and regulations established for each particular Event, including speed zones, testing times, and testing only in designated areas, if applicable. The teams shall be solely responsible for checking with the appropriate UIM Officials at the Event during registration as to the applicable rules and regulations for testing at the event. The team must obtain the Race Instructions.

Any telemetry/locator GPS/Data or TV/Video recorder(s) required by the UIM to be fitted to the race boats must remain in place and removed or its content reviewed only with the permission of the Technical Director.

Violation of this rule may result in penalties including, but not limited to, fines, time penalties, disqualification and suspensions of not less than 30 days.

Ignorance of the rules is no excuse for not complying with the rules.

04. INTERPRETATION OF THE RULES

In all cases not foreseen by the international rules or seemingly inaccurately defined, any decision has to be taken bearing in mind the spirit of the rules. Decisions on interpretation will be taken by the Race Director and /or the Race Jury or the UIM.



Any telemetry/datalogging device or GPS transponding and/or TV/Video equipment required by the UIM to be fitted to all competing race boats is deemed to be the official technical recording of the race. Team owned devices are not accepted as evidence.

Television/Video evidence may be used for either the correction of a race decision, or implementing a change to provisional results up to 5 days after the posting of results.

#### 05. AWARDS AND PRIZE MONEY

No prize money shall be distributed to any driver until protests have been completed and the official order of finish is posted by the race committee in that class.

Any boat which receives prize/start money and has an outstanding debt with UIM/Event Promoter will have that debt taken out of the prize/start money that is due them. If fees are paid by cheque at the race site, prize/start money may not be paid until the cheque clears.

The sponsoring organisation may designate special awards for outstanding performance.

A minimum prize fund may be established and announced in the Race Instructions.

Any X-CAT awards provided by sponsors have to be agreed with Series Promoter.

Trophies will be awarded to the first three finishers. It is the responsibility of the competitor to accept their trophy at the awards ceremony or have a representative accept on their behalf. UIM will not be responsible for the trophy after the event.

All rules in the Rule Book, and all requirements of sponsors must be met, in order to participate in the prize fund or start monies.

#### 06. PAYMENT OF FINES

All fines must be paid to the UIM by the end of the race. If the fine has been issued in conjunction with penalties qualifying for appeal, then the appeal must be written and properly filed within the specified time limit for appeals. Fines that remain unpaid after 14 days shall be subject to additional penalties. Post race/event issued fines must be paid within 14 days of notice being issued or will become subject to additional penalties.

#### 07. CONSUMPTION OF ALCOHOL

No contestant shall participate in a race / practice or any Media-Hospitality Promotional Official Run, nor shall any official serve in an official capacity, while under the influence of intoxicants, or any dangerous drug, which shall include narcotics, depressants, stimulants, or hallucinogenic drugs. The race officials (Officer of the Day or the UIM Commissioner) shall suspend for minimum of six (6) months any competitor or official whom he believes to be under the influence of intoxicants or dangerous drugs as defined above or during the time the competitor arrives at the race site, on race day, until the competitor has been cleared through post-race inspection and for one hour after the race is over. No alcoholic beverages are to be consumed until one (1) hour after the unofficial results are displayed. No contestant or official participating at a UIM event may evade this rule by withdrawing his entry or by resigning his duties while at the event. Ceremonial champagne or equivalent usage in the winner's circle shall be permitted provided that only minor consumption occurs.

A competitor shall, if requested by an authorised officer, submit to an alcohol test. Written notification of selection for testing will be handed to a competitor by an authorised official. Refusal or failure to do so may be taken as if a positive test had been ordered and dealt with accordingly.

Testing when carried out shall be using an Alcometer.

For International Races, any competitor found to have more than BAK 0,10 0/00 (0,10 g/L blood // AAK 0,020 mg/L air) 1 hour before and during testing or race shall be immediately suspended and disqualified from the whole event. A format report shall be forward to the N.A. of the competitor.

If in an Organising Country, the legal requirements of the above numbers are lower, then these numbers have to be announced in the advance regulations and will prevail.



## 08. ANTI-DOPING REGULATIONS

The Anti-doping regulations are in line with the World anti-doping code as published in the UIM Offshore Rule book.

## SECTION B

### 09. INDIVIDUAL REQUIREMENTS FOR PARTICIPATION IN AN EVENT

**Releases** - All participants in each event must execute a liability release to UIM, prior to racing. No owner, driver, contestant or his representatives or any race official shall hold any other owner, driver, competitor or his representatives or any race official liable for any personal injuries or damage resulting from an accident occurring during a sanctioned race, except as the result of a deliberate collision or premeditated act of unsportsmanlike conduct. The interpretation of this shall rest with the Race Director subject to review by the UIM.

**Suspension, Expulsion** - No individual who has been expelled from the UIM or its events or who is currently under suspension by UIM shall be permitted to officiate, serve as a race committee member, act in any capacity of assistance, or participate as a boat owner, driver or crew member in any sanctioned racing event. No boat belonging to that individual shall be allowed to compete, nor shall it be chartered or otherwise transferred except by final sale to render it eligible for a sanctioned race. The UIM also reserves the right to deny future membership to anyone who has previously been expelled or suspended from any category of UIM.

**Medical / Rescue, Tow and Patrol Boat Coverage - LIFE JACKETS/ BUOYANCY SUITS** - During all official times, all assistance / safety / rescue boat teams and all teams in support boats must wear life jackets and to conform to the LOC/NA safety regulation or special prescriptions.

The race organiser may or may not provide medical and rescue coverage for testing prior to the race event. If medical and rescue coverage is to be provided, a schedule will be posted at Race Registration detailing times and specific location of covered testing.



# PART 3

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## EVENT RULES

The following General Racing Rules apply to all UIM-sanctioned X-CAT Series events.

The Rules and/or Regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all X-CAT Series events and by participating in the events, all UIM members are agreed to abide by these rules. No expressed or implied warranty of safety shall result from publications of, or compliance with these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to participants, spectators, or others.

All Race event rules and criteria/requirements may be subject to venue specific requirements and National Authority or geographic conditions that result in changes to suit as contained in race instructions, bulletins or changes on safety concerns.

### SECTION A

#### 01. RACE ORGANISATION AND SANCTION

##### 01.01. ELIGIBILITY

In order to be eligible to organise a UIM X-CAT Series event, a Local Organising Committee must be sanctioned by the X-Cat Promoter (WPPA) and recognised by the UIM. The National Authority or the Local Organising Committee must be recognised by the UIM, which must authorise the event, ensuring that the minimum requirements established in mutual agreement with the X-CAT Promoter are in place.

##### 01.02. RACE DATES

Candidatures for any X-CAT event (World – Middle East- Continental Championship or any event using the name “Extreme Cat / X-CAT”) must be addressed to the X-CAT Promoter (WPPA). Event dates will be agreed between the UIM and the X-CAT Promoter, which must present the annual calendar in accordance with a timescale to be agreed with the UIM.

Final approval for Race events must be obtained from the UIM after been approved and submitted by the Promoter.

##### 01.03. SANCTION/EVENT HOSTING FEES

The X-CAT Promoter is responsible for the definition of all aspects relating to the contract with the X-CAT Local Organising Committee.

The financial conditions for staging the X-CAT event will be established between the X-CAT Promoter and the Local Organising Committee.

The X-CAT Promoter and the UIM will establish, on an event-by-event basis, the fee due to the UIM, which will not include the event insurance required or the cost of UIM officials assigned to the event.

##### 01.04. REQUIREMENTS

The Local Organising Committee contracted by the X-CAT Promoter must follow the requirements and criteria established in these rules with regard the organisational and safety aspects of the event.

#### 02. TITLE

The title of the Championship will be known as the “UIM X-CAT World Powerboat Series”.

#### CHAMPIONSHIP/ SERIES

#### 03. SERIES POINTS & SCORING SYSTEM

In any of the X-CAT races, the points will be awarded to the first and second named pilots only. The first and second named pilots must be the helmsman, throttleman or navigator. The Navigator cannot be the first named pilot.

Unless otherwise specified, the following scoring system is to be adopted:

1 - 400 pts	11 - 22 pts
2 - 300 pts	12 - 17 pts
3 - 225 pts	13 - 13 pts
4 - 169 pts	14 - 9 pts
5 - 127 pts	15 - 7 pts
6 - 95 pts	16 - 5 pts
7 - 71 pts	17 - 4 pts
8 - 53 pts	18 - 3 pts
9 - 40 pts	19 - 2 pts
10 - 30 pts	20 - 1 pts

For events with more than 20 entries, the following bonus points should be given, so that all drivers can get points.



#### 03.01. 21-25 BOATS: 5 BONUS POINTS

Position 1 to 20: should all have 5 extra bonus points

Position 21: 5 points

Position 22: 4 points

Position 23: 3 points

Position 24: 2 points

Position 25: 1 point

#### 03.02. 26-30 BOATS: 10 BONUS POINTS

Position 1 to 20: should all have 10 extra bonus points

Position 21: 10 points

Position 22: 9 points

Position 23: 8 points

Position 24: 7 points

Position 25: 6 points

Position 26: 5 points

Position 27: 4 points

Position 28: 3 points

Position 29: 2 points

Position 30: 1 point

#### 04. CHAMPIONSHIP

The X-CAT World Series will be determined from the points accrued from all the X-CAT World Series events. All Races carry points with no discards.

##### 04.01. CHAMPIONSHIP TIES

In the case of a tie in the overall Championship/Series, the number of first places shall be considered, then the number of second places, etc.

In the case of the Championship/Series still being a tie, the boat with the fastest average speed in any of the Series races will be deemed the Champion.

#### 05. CREW/BOAT COMBINATION

The points and penalties for the UIM X-CAT World Series will be awarded in principal to the Registered Boat number and Team entry.

It is mandatory to register the Team Owner Name before the start of the Series (Team Name - Boat number & Crew) eligible to race.

##### 05.01. NATIONALITY

The nationality of the entry is that determined by the Team/Owner as declared at the first race. The nationality of the Pilots is as declared at the first race and whose nationality is determined by either the Passport or Racing Licence of that pilot.

### SECTION B

#### LOC ADMINISTRATION/ORGANISATION

##### 06. RACE COMMITTEE MEMBERS AND ROLES

The Local Organising Committee appoints a Race Committee which is composed of:

- Local Race Officer
- Safety Officer
- Jury Boat Coordinator
- Course Officials
- Pit Officer
- Event Secretary



#### 06.01. LOCAL RACE OFFICER

The Local Race Officer forms the principle link with the local organisational race management safety, Jury boats, air safety etc.

The Local Race Officer must direct the organisation of the races on behalf of the Local Organising Committee.

Good knowledge of the UIM Rules, especially those relating to X-CAT / Offshore Power Boat Racing.

Fluent written and spoken English are obligatory.

He is responsible for the implementation of local services and must maintain communication with Jury boat coordinator and local safety officer.

#### 06.02. LOCAL SAFETY OFFICER

The Safety Officer reports directly to the Local Race Officer. He/She coordinates with the X-CAT Safety Officer, in accordance with the UIM Safety Procedures and Guidelines (Appendix 1), for all safety and rescue activities on land, on the water and in the air, including the preparation necessary for these activities.

During all official race activities the Safety Officer must be present at Race Control.

#### 06.03. JURY BOAT COORDINATOR AND COURSE OFFICIALS

This group is composed of one Coordinator located at Race Control, plus a minimum of two observers for each turn - buoy. The group must attend training by a qualified UIM Official a minimum of two hours prior to Pole Position.

At the end of the race, they draw up and sign a statement certifying that all participants' conduct during the race and at the turn marks was in accordance with the UIM On-Water Procedures and Guidelines (Appendix 2), UIM Turn buoys clarification drawings (Appendix4) and with the Race Instructions. This statement is handed to the Race Director, who will apply the UIM rules in the case of infringements. The Course Officials monitoring turning marks must have a clear, unobstructed view.

#### 06.04. LOCAL PIT OFFICER

The Pit Officer must guarantee security of the areas and all the services as detailed in the PIT REQUIREMENTS under item 08.01 and the UIM X-CAT On-Land Procedures and Guidelines - APPENDIX 3.

#### 06.05. LOCAL EVENT SECRETARY

The Event Secretary is responsible for the general local organisation of the event. He/She must ensure that all the various local officials know their respective duties and that they have the necessary documents and equipment to fulfill their duties. He/She must collate all documents following the race.

#### 07. EVENT INSURANCE

The Organising Committee must have an Insurance Policy which covers all Race and Non-Race activities for both Competitor and Organiser liability. It must include event third party insurance which is to cover all racing and non-racing activities and, in particular, any damage resulting from towing or launching of the boats and full third party cover for all race officials and associated race helpers.

The Organiser's insurance will be available from the Organising Committee during Race Administration.

The competitors are strongly recommended to have a racing third party insurance. The amounts must not be inferior to those required by the law of the countries where the races will be held. In addition to any other insurance as required by the National Authority or local organizer, Fee to be paid by the team.

All insurance must cover the whole period of the Practice, Pole Position and Races from beginning to end of the event.



## SECTION C

### RACE ADMINISTRATION

#### 08. RACE REQUIREMENTS

The latest version of all UIM Procedures and Guidelines, as listed below, must be strictly adhered to:

- APPENDIX 1 - Safety Procedures and Guidelines
- APPENDIX 2 - On-water Procedures and Guidelines
- APPENDIX 3 - On Land Procedures and Guidelines
- APPENDIX 4 – Turn Buoys Clarification Drawing
- APPENDIX 5 – Start Procedure Drawing & Clarification
  
- APPENDIX 6– Advertising and Branding Code

Failure to comply with the indications given therein will result in a penalty to be applied at the discretion of the Race Director.

##### 08.01. PIT REQUIREMENTS

At all Events the Local Organiser must establish clearly defined, readily identifiable areas for Dry Pits, Wet Pits, and Crane/Launch Areas in accordance with the outline given below.

In all cases these areas must be set apart from the general public by being fenced off or some other means, except that the public may be admitted to the Dry and Wet Pit Areas during designated times. Only contestants, crews, officials, approved event staff or current members of the UIM or the X-CAT Promoter shall be admitted to the Launch Area. The sale to the public of pit admission tickets or passes to any pit area is strictly prohibited. These areas require the execution of Insurance Waiver/Indemnity by all participants (racers, crews, family members, officials and event staff - not spectators) for admission. Alcohol use within any of the designated pit areas by any UIM/X-CAT Promoter members or a signee of the insurance Waiver is strictly prohibited and may result in the imposition of a penalty at the discretion of the UIM events have three mandatory pit area classifications with the following restrictions:

Crane and/or Launch area and/or Fuelling area: shall be defined as an adequate area around launching cranes, fuel storage, fuelling area. Entry into these pit areas requires UIM or X-CAT Promoter Racing, Crew or Official Membership. Except for cases of emergency, the maximum number of boat crew in the lift and drop areas is restricted to maximum 4 support crew and 2 competitors. Team guests are not to enter this area.

Fuelling of boats shall be allowed only in the approved fuelling area(s). Fuelling or de-fuelling of race boats in the water is specifically prohibited. Fuelling at commercial marina pumps on the water or commercial land-based filling stations is allowed, providing they have met the fuel requirements (as specified in the Official Advance Notice & Race Instructions); it is also mandatory to use any fuel supplier provided.

Enclosed Footwear is recommended in the Craning Areas.

Waste oil and fuel must be disposed of in a responsible manner by all race teams.

Signs shall be posted around all working pit areas designating "No Smoking or Drinking of Alcoholic Beverages."

All UIM race events are required to provide crane service for boats that provide their own single point lifting harness. All boats are required to provide their own single point lifting harness for weighing purposes.

The crane area shall be highly restricted and fenced off from all other areas. No persons shall be permitted to the crane area except specifically authorised personnel.

Wet Pits - Wet Pits shall be defined as the docking areas, as crew working areas and, except in cases of emergency, the maximum number of boat crew in wet pit areas is restricted to 4 support crew and 2 competitors. Team guests are not to enter this area. Access to this area may be available by controlled admission pass dependent on pontoon capability.

Dry Pits - Dry Pits shall be defined as those areas where race boats are on display prior to launch. Race Officials have the right to remove a race team or race boat from this area for disruptive conduct. Teams may be subject to other disciplinary actions.



## 09. ADVANCE NOTICE

The Advance Notice must be received by the UIM and, after the approval of both the X-CAT Promoter and the UIM, it must be sent to the X-CAT Promoter's Office for distribution at least 30 days before the Race.

The Advance Notice shall contain the following information:

- Local Organising Committee contact details
- Provisional timetable
- Race course and distances
- Map of Pit Areas
- Accommodation, travel and transport details for teams and equipment
- Details of insurance, fuel and social events, if this information is available

## 10. RACE INSTRUCTIONS

The information contained within the Race Instructions is to be considered as a clarification of the UIM Rule Book or additions to take account of special local conditions.

These Race Instructions shall contain all of the following information:

- Official timetable and locations.
- Official race course and details.
- Local conditions, such as any special instructions and/or information pertinent to the race, e.g. special radio channels required, nautical chart numbers, tides and any venue-specific Authorities' instructions.
- Terms and conditions of insurance for personal and material damage to third parties, competitors, to be included if available.
- The minimum limit of insurance cover required.
- The type of risks to be covered by the insurance.
- The amount of insurance fees to be paid at the race site.

No course, timetable or race site logistics can be published without direct approval of UIM and the X-CAT Promoter.

## SECTION D

### 11. RACE MANAGEMENT AND OFFICIALS

The following UIM officials will be present at all Events to assist in the management and conduct of the Event:

- Race Director (RD)
- Safety Officer
- Assistant Race Director / Race Commissioner
- Technical Director
- Race Administrator

Each of the above may have an assistant, to be appointed from the pool of UIM-approved personnel.

#### 11.01. RACE DIRECTOR

The overall on water management of the race is the responsibility of the RD who liaises with the (his assistant - RD - and the Safety Officer).

The RD keeps an overview of race and is the ultimate decision maker on the overall conduct of the event. This includes such matters as to whether the weather conditions are suitable for racing, while the RD is responsible for the actual conduct of the race. It is therefore important that the RD is a very experienced Official and that he is recognized as such. The RD also supervises the onshore aspects of race management.

The UIM O.P.C shall appoint a Race Director who shall serve at the discretion of the UIM. The Director shall be responsible for the supervision of the race and the correct application of the UIM rules. He shall perform such other duties as are requested from time to time by the UIM. The Race Director must be included in the race/event approval process and receive all course and race instruction detail for approval.



Presence at Events - Each UIM-sanctioned racing event must have a Race Director present. Should an officially designated Race Director be unable to attend any sanctioned racing event, the UIM may appoint an alternate to serve at that racing event.

#### ***Duties and Responsibilities***

Pilots' Briefings - The Race Director shall attend all pilots' briefings. He shall see that all questions relative to racing rules are properly answered and that the conduct of the race itself is in accordance with all racing rules.

#### ***Enforcement of Rules***

**Purpose** - The Race Director shall see that all UIM racing and technical rules are enforced throughout the event. All possible rules infractions shall be reported only to the Race Director by the appropriate UIM official. It shall be the sole responsibility of the Race Director to communicate with any team member or other party who has been charged with the violation of any rule, as well as the proposed action that UIM shall take as a result.

**Multiple Roles** - Unless there is prior agreement and approval by the UIM, the Race Director shall not serve in any other official capacity such as Timer, Technical Director, Jury Chairman/Member etc., nor shall he act as a member of any committee charged with the actual management of the racing event or any other racing organisation that directly competes with the UIM while also serving as the Race Director.

**Interpretation of Rules** - He/She will along with the Assistant Race Directors be responsible for overseeing all sporting matters or penalties of a Championship / Race nature outside the remit of the race jury. The Race Director will additionally chair and coordinate event meetings between race officials, promoter and local authorities representatives on event. Should any questions regarding the interpretation of racing rules arise, the Race Director shall rule on these matters after consultation with all contestants, witnesses and other UIM and local officials involved. In the event that the Race Committee or another UIM Official makes an erroneous decision or interpretation, the Race Director is authorised to overrule that decision.

Training - Members desiring to become a UIM official must complete a training programme by the UIM.

Other - The Race Director of a sanctioned event also must ensure that:

- All proper officials be present and function during the conduct of an event to the relevant UIM standard.
- The approved course is provided (courses must be pre-approved by the UIM before the sanctioned testing or racing begins).
- At race sites, the UIM safety and rescue requirements are met.
- All bulletins, results when he also fulfils the role of the Local Race officer (OOD) - and official race documentation are signed by him, and that he is on hand at all times for this purpose.

**Post Race Reports** – The Race Director must file a report in writing to the UIM detailing all of the events, the conduct of the races, any accidents or other incidents such as disciplinary actions, technical Disqualifications, weather conditions at race and test times, course infractions, and penalties assessed interviews with competitors or witnesses to infractions, etc.

#### 11.02. SAFETY OFFICER

The Safety Officer is appointed by the X-CAT Promoter and recognized by UIM O.P.C, is responsible for safety and rescue operations. He must be familiar with the race venue, with the characteristics of the class(es) competing powerboats and any applicable governmental or similar rules. The cooperation with local or private non-profit rescue organizations is highly recommended.

The Safety Officer must be familiar with the safety regulations under which the race is being organise LOC, that is to say the safety requirements of the National Authority, of the Class rules, of the Race Instructions and of any authority over the race water such as the local harbour master/board.

It is highly desirable that any possible conflict between these be resolved before the race and that the Race Instructions give the final word, including resolution of any conflict.

#### 11.03. RACE COMMISSIONER /ASSISTANT RACE DIRECTOR(s)

The Race Commissioner /Assistant Director(s) will be appointed as chairman of the Protest Jury. The Commissioner/Assistant Race Director will assist the role of race director where necessary in his/her absence. The assistant acts with the director in the delivery and coordination of race rules and the local services.

The Assistants are also course race officials with regard to race rules, and may be acting as Director within race control, or as Start Marshal or Course Judges.

Post Race Reports – The Race Commissioner shall file a report in writing to the UIM detailing all of the events, the conduct of the races, any accidents or other incidents such as disciplinary actions, technical Disqualifications, weather conditions at race and test times, course infractions, and penalties assessed interviews with competitors or witnesses to infractions, etc. The Assistant Race Director shall report all accidents, even when no injuries are involved, to the UIM. This does not stop local authorities from reporting those accidents, as required by any local authority law or otherwise to be reported. Race Officials will assist local authorities in completing their reports where necessary.



#### 11.04. TECHNICAL DIRECTOR

The UIM O.P.C shall appoint a Technical Director who shall serve at each event. The Technical Director shall be responsible for developing and implementing the Technical Inspection plans, procedures and protocols (including branding marketing requirements) for each Event, and managing the Assistant Technical Director (if any) and their implementation of those practices, procedures and protocols.

The Technical Director is a member of the UIM O.P.C. He's responsible for the research into future technical rules and requirements, and is empowered to liaise with manufacturers to facilitate the future technical advancements.

Assistant Technical Director - The Technical Director shall appoint from time to time as necessary additional qualified assistants to serve in conjunction with him or in his place. They shall be charged with specific functions as determined by the Technical Director. A list of such Technical Assistants may include suitably qualified individuals from a UIM-recognised National Authority.

Presence at Events - Each UIM-approved racing event may have an Assistant Technical Director present who must be appointed by the UIM O.P.C upon the request of the Technical Director.

##### **Duties and Responsibilities**

Compliance with Technical and Safety Rules - The Technical Directors and assistants main responsibility is to inspect the competitors' racing equipment for compliance with the applicable technical, equipment and safety rules and document their findings on each piece of equipment inspected at an Event.

Unsafe Equipment - The Technical Director shall also have the right to refuse any entry at any time prior to the start that in his/her opinion is not designed, built and equipped to the standards of seaworthiness and safety required for any adverse weather and water conditions during the race.

Further Technical Responsibilities - It is also the Technical Directors responsibility to check all the activities relating to technical areas (craning, fuel, etc.) and to report his findings to the Race Director.

Reporting Infringements - The Technical Director inspect racing equipment for compliance with the technical and safety rules and if they find what they believe to be a violation of any of those rules, then they shall report the infraction to the Race Director for the event. The Technical Director do not discuss their findings with anyone including any member of the relevant team, until the matter has been reported to the Race Director and the Race Director has authorised same. Extra Duties in the Case of Accident - In the case of accident, the Technical Director must check all race boats involved for seaworthiness and report his findings to the Race Director.

#### 11.05. RACE ADMINISTRATOR

The UIM shall appoint a Race Administrator. He/She is responsible for the general sports-related administration and organisation connected to the event, and shall also perform such other duties as requested from time to time by the UIM. The Race Administrator shall be responsible for managing the assistant administrator (if any).

##### **Duties and Responsibilities**

Membership, Registration at the Event - The Race Administrator shall be responsible for collecting and accounting for all registration details along with the respective forms.

Insurance Disclaimer - It shall also be the duty of the Race Administrator to have all members sign the insurance Disclaimer at each race site.

General documents and Race Bulletins - The Race Administrator will supervise the Local Event Secretary in organising and drafting all general racing documents and bulletins as required. The Race Administrator will also organise any briefings/daily meetings as required.

Signature and Posting of Results - The Race Administrator shall be responsible for posting the arrival order in a pre-determined and announced location, and communicating those results to the designated media representative following the final race of the day.

The Race Administrator shall also be responsible for arranging for the final results to be signed and for posting them on the Official Notice Board.





## 11.06. OFFICIAL RACE TIMING

An Official timekeeping system and operator will be appointed by the UIM and is the official race timekeeping system. In X-CAT the system and operator will be provided by the X-CAT Promoter. Official timekeeping system may be linked to the mandatory transponder equipment required by the UIM. The mandatory transponder/virtual system may be used as either the principle timing system or the reserve system and operated by the UIM and/or its contractors.

The UIM Race Officials will provide to the operator the necessary entry list details and the official course distance calculations and information.

The Race Director and his assistants will provide information for the operator regarding race information, penalties, communications etc. as necessary during the course of each session and race.

At the end of each on water timed session the operator will make available to the Race Administration the system results.

Race Administration will publish the official timed and signed result.

These results are all provisional and subject to post race technical scrutineering, post race/pole position reports, protests etc.

## 12. RACE REGISTRATION (ON-SITE)

Registration will take place at the Race Administration office.

Race Registration times will be as per the Advance Notice. All Race Registration requirements (including payment of insurance) must be completed by the time Race Registration closes for signing in to the event.

Failure to do so will result in non-participation in the racing event.

On the registration form for each X-CAT Race, the names of the Team Owner, Team Manager, first and second pilots, together with reserve and test pilots, must be specified. Also the name and boat number and a copy of the current valid measurement certificate with registration number must be supplied.

On signing in at Race Registration, a Scrutineering/Technical inspection slip will be issued, which will enable competitors to present their boats for Scrutineering/Technical inspection.

All Official Race Information, Bulletins and Results will be posted on the Official Notice Board at the Race Registration Office.

Competitors will be provided with identification tags/wrist bands, if required, which must be worn at all times during practice, Pole Position and Racing.

Any person other than a racing crew member wishing to embark on the boat for practice will only be authorised under the condition that they are a UIM-licensed reserve/test pilot and have registered as such during Race Administration or as otherwise agreed by the UIM Race Director.

Changes to crew must be notified to race control a minimum of one hour prior to testing. PENALTY Euro 600.

He must sign up with the Organiser's Insurance Agent with either the Local Organiser or the X-CAT Promoters' representative.

He must be in possession of all current documents required by the UIM and organising committee.

All race numbers will be between 1 and 99 and will be allocated by the UIM after the entry has been accepted. Number 1 will be allocated to the previous year's World Champion, unless otherwise requested. The replaced number of the World Champion's boat will be reserved for this boat to resume when it is no longer the World Champion. Race Bulletins will be issued and signed for at Race Briefings and/or the Race Administration Office.

Restraint Indemnity - UIM rules mandate the signing of the National Indemnity form regarding restraints. In view of the International nature of the X-CAT series and the fact that all boats use restraint systems, in applying for the licence, the competitor confirms that he has read and understood the UIM, and his National Authority's rules regarding restraint systems and confirms that the boat entered for the event in which he is taking part has been constructed to and conforms with these rules. He further undertakes not to hold the UIM, his National Authority, the Organising Club, or any of the servants or agents of the foregoing, nor any other person connected with the organisation of an event, responsible for any personal injury incurred by his wearing this restraint system. He will also produce on demand a copy of his National Authority's rules for restrained cockpits.



### 13. PRE-RACE INSPECTIONS

#### 13.01. REQUIREMENTS

The Technical Director may also at any time during the event pre testing, pole, and/or race, require random inspection and testing of equipment for compliance to the rules.

All race boats entered in a sanctioned race are subject to a pre-race inspection by a Technical Director.

The time and place of such inspection shall be published in the Advance Notice for the Event.

No race boat may be considered a bona fide entrant in a UIM race until such time as the Technical Director has passed and signed the official pre-race technical inspection form.

It is the responsibility of the Team Owner or Manager to submit his team's equipment to the Technical Director for his inspection. If, in the judgment of the Technical Director, a boat and/or safety equipment is unseaworthy, unsafe, or unmanageable, the non-compliance must be brought to the attention of the Race Director. If the Race Director determines that the condition cannot be rectified prior to the start of the race, then he shall have the right to prohibit the boat from competing.

The Technical Director shall examine each entry for compliance with all safety requirements and shall also visually inspect hull, propulsion and engine for compliance with X-CAT Technical Rules. The spirit as well as the letter of these rules shall be enforced equitably to all entrants.

### 14. PRE-RACE TECHNICAL RULES VIOLATIONS

#### 14.01. NOTICE TO OWNER

In the event that a pre-race inspection finds a technical rule violation, the Race Director or Technical Director must notify the Team Owner of the boat or the Team Manager of the specific nature of the alleged violation as soon as practical, but in no instance later than the Final Pilots' Meeting. Depending upon the nature of the violation, and whether this is a first or multiple offence, the UIM may issue notice with a range of penalties from fines to immediate Disqualification from the event.

#### 14.02. CORRECTION OF VIOLATIONS

After a race boat owner has been notified of the violation, he shall have until 1 hour prior to Pole Position to correct the violation, unless he has been disqualified. Should he elect to correct the violation then it shall be his responsibility to have the boat re-inspected and cleared for competition.

#### 14.03. PROTEST & APPEALS (Refer to Article 400 of the UIM Rule Book white pages and any amendments as described in the Race Instructions)

#### 14.04. GENERAL INSPECTION

During this Inspection the UIM shall also check the team for compliance with the marketing requirements in force for the race series or event venue. Teams shall comply with the marketing requirements as set out in their contract with the Series Promoter.

### 15. TEAM'S REPRESENTATIVE

A Teams' representative must be elected by the X-CAT Teams Owners in the presence of the UIM Official and candidates must be teams actively participating in X-CAT Series for a minimum of two complete season.

The team's Representative is elected at the first race of the season for a one-year term.

The Teams Representative has a seat in O.P.C with voting right.

### 16. DRIVERS' REPRESENTATIVE

A Drivers' representative must be elected by the X-CAT Drivers in the presence of a UIM Official. Candidates must be drivers actively participating in X-CAT Series for a minimum of two complete season.

The Drivers' Representative is elected at the first race of the season for a one-year term. He/She should be available to attend meetings called by the Promoter at annually, to report and advise on the consensus recommendations and safety requests from the drivers. He may be invited to attend all UIM-O.P.C meetings and may receive all relevant UIM communications.

### 17. TEAM MANAGERS

Each boat must designate its Team Manager at each race venue for contact in case of an accident or Force Majeure this representative must be present and registered at Race Administration.

Similarly, each boat must have its representative present at the Team Managers' Area (as described in the Race Instructions) throughout Official Practice sessions, Pole Position and Grand Prix races for all official communications. PENALTY Euro 500.



It is mandatory to have at all times during on water activities an efficiently working radio system, and to stay in permanent radio contact with Race Control. PENALTY Euro 500.

Team Managers' briefings (if planned as per Race Instructions) must be attended by the Team Manager. It is his responsibility to ensure that all team and crew members are informed about all local authority requirements both on land and on water. He must also ensure that the crew are fully conversant with all testing, pole position and race course areas and all relevant safety requirements.

## SECTION E

### 18. PILOTS' BRIEFINGS

#### 18.01. ATTENDANCE

Pilots' briefings shall be confined to crew members, Team Owners / Managers and concerned officials only. Unless otherwise provided in these rules attendance at these meetings is mandatory for every boat crew. All pilots and crew members attending any pilots meeting must wear a team uniform.

Racing Team members who fail to wear a uniform as specified above will be subject to a fine of Euro 200.

#### 18.02. PILOTS' BRIEFING TIMES

It is mandatory for one pilot from each team to attend the Teams course inspection at all venues. Time to be announced in race instructions.

It is mandatory for both first and second pilots to be present at the main briefing and 1 of the 2 nominated pilots for the weather briefing (if held).

Signing in for any briefing will begin 20 minutes before scheduled pilots' briefing time. It is mandatory for all competing crew members to attend and sign in at the briefing. All signing in must be completed prior to the pilots' briefing time. Penalty for non-attendance - Euro 700 fine per person and / or Disqualification. Pilots who are late for a briefing: Penalty Euro 350.

It is essential that all competitors read and are totally aware of the contents of the Race Instructions and/or any Race Bulletins before attending the Pilots' Briefing.

It is the responsibility of all pilots to bring the Race Instructions and any bulletins to all Briefings.

Event Race Instructions may contain one mandatory Official Reception that competitors must attend. Penalty Euro 700.

The use of mobile phones during briefing is strictly forbidden Penalty Euro 150.

#### 18.03. ALCOHOLIC BEVERAGES

Sale or consumption of alcoholic beverages is strictly prohibited at all pilots' briefings.

#### 18.04. UNIFORMS

All racing crew and team members are required to be in team uniform at Pilots' Briefings, race/event functions and on race day, unless other requirements are published in the Race Instructions / Programme. Penalty Euro 200.

## SECTION F

### 19. UIM SUPER LICENCE

In order to participate in X-CAT Series, every crew member of the boat must hold a UIM X-CAT Superlicence.

All UIM licences are valid from the date of issue to 31st December of the same year.

In the event of any accident either within the sport or beyond it's jurisdiction that prevents an individual from racing on medical grounds, a new post injury medical certificate must be submitted prior to re issue of the individuals licence.

Additionally in the case of a race incident resulting in medical care the competitor must be signed fit to return to racing by the race/event doctor. If this is not obtained, a signed declaration from a Doctor of fitness to return must be submitted to the UIM.

The event insurance must be complied with.



#### 19.01. ISSUE OF LICENCE

A UIM X-CAT Superlicence can be obtained either following a request by the pilot's National Authority to the UIM.

The application for a UIM X-CAT Superlicence must be lodged with the UIM four (04) weeks prior to the first event in which the pilot wants to participate is held.

The price of the UIM X-CAT Superlicence will be Euro 150. After the four weeks deadline for the pilot's to apply for the UIM X-CAT Superlicence via their National Authority, late penalty fee of Euro 300 will apply.

All required documents must be completed and the following additional documentation, or proof of such, must be provided to the UIM Secretariat:

- The sports career of the pilot, including the results obtained in the previous two seasons of offshore racing.
- Evidence of having raced during the previous 2 years in Extreme Cat (6 litre), or equivalent as pilot or throttleman in at least 7 races. (Other classes may be taken into consideration as agreed by the UIM).
- Current immersion certificate.
- A medical certificate providing all the data required by the UIM Official medical form. (This form is available with UIM Secretariat)
- Two photographs (passport size).
- The fees fixed by and paid to the UIM.

The UIM may issue the UIM X-CAT Superlicence on the basis of the information supplied by the pilot's National Authority. The UIM may withdraw a UIM X-CAT Superlicence on the recommendation of the relevant pilot's National Authority and the agreement of the X-CAT Promoter.

Any pilot who has passed their 50th birthday before the date of application for a UIM X-CAT Superlicence will be obliged to submit a medical report from their own doctor, and also a report of an examination by a doctor nominated by the UIM.

The minimum age for a pilot to qualify to hold a UIM X-CAT Superlicence and to compete in a X-CAT race is 18 years unless otherwise agreed by the UIM.

The application and nomination by the pilot's National Authority will be deemed to be the National Authority's consent to represent their country for the duration of the UIM X-CAT Superlicence. Should the NA not wish a UIM X-CAT Superlicence holder to represent his country at any time, the NA must inform the UIM in writing.

The UIM may authorize a provisional Licence for the second Driver who may not have the above - mentioned sports career to participate in the competitions provided that:

- The First Driver is well - experienced, has attended the last two C-CAT Series and takes the responsibility of the Second Driver's behavior in the race, the first driver must also sign the UIM letter of agreement regarding responsibility and ;
- He should have an Offshore Licence issued by his relevant National Authority.
- The application should be should be authorized by his N.A.

#### 19.05. COCKPIT EVACUATION / IMMERSION TESTING

Before racing in a craft with restraint systems, all crews must have passed in the last fourteen months, an immersion test in a restraint system to ensure that they can exit a reinforced cockpit crew compartment successfully.

Prior to taking the immersion test, all crews must have a valid scuba certificate or have received suitable training. This alternative training should be approved by the National Authority.

An Immersion Certificate to certify the passed test, showing the expiry date, must be delivered by experts recognised by a National Authority.

All riding crew members using restraints must sign the National indemnity form prior to competing in any race or practice.

UIM X-CAT Cockpit Evacuation training and testing is available at the X-Cat Series races.

Regardless of any current immersion training certificates held by competitors the UIM X-CAT Cockpit Evacuation training and testing is mandatory.

Immersion testing is to include training for all crews in both self extraction and full partner rescue training utilizing on board air systems. All crew members must be capable of full use of on board air systems to the satisfaction of the appointed UIM testing facility.

The UIM will also request mandatory random testing throughout each racing season for all crews, where possible this will be integrated with the local event rescue crew training.



## 20. PARTICIPATION RULES

The UIM and its National Authorities and Organising Clubs reserve the right of entry of any team or competitor and has the right to accept or reject entry/eligibility at the discretion of the UIM on the grounds of the unsuitability of any entrant for reasons such as and not limited to unsportsmanlike conduct, bringing the sport into disrepute.

Eligibility - The Team entries for the Series/Championship must be received by the UIM via the X-CAT Promoter and notified to the relevant recognised NA and must include at least the Owner's name, the name of the Team Manager, the names of the First and Second Pilots and the race number of the boat. The name of the two pilots must be stated including any reserve/test pilots if any.

It is mandatory for each entrant to participate in all X-CAT races to accumulate points unless otherwise agreed in writing by the UIM.

The UIM will decide the penalty which may be a fine of min. Euro 5,000 max. Euro 35,000 and/or alternative penalty for contravention of the above.

In the case of accident damage to a boat, the pilot may apply to the UIM for dispensation not to compete in the next races until the damage is repaired. The Technical Director must verify and certificate the damage and report to the UIM. If the dispensation is requested for damage outside the races, the Technical Director must verify the boat where it is situated. The applicant must pay all the costs.

New series entries arriving after the first round will not be eligible for Series/Championship points but will be podium placed. Late entries are at the discretion of the promoters and the UIM only.

Inscription Fee & Prize Money - as per contracted promoter's agreement.

Entry to the UIM X-CAT Series must be made on the official UIM/WPPA entry form with the accompanying Race Number registration fees prior to racing and include the names of the first and second pilots. Also the teams request for allocation of race number.

Race numbers are allocated as follow:

Race Number 1	Reserved for Current Champion (Unavailable if not registered by the last years Champion)
	Fee: Free
Race Numbers 2-9	Fee: Euro 1050
Race Numbers 10-99	Fee: Euro 550

Previous holders of numbers have the first option to keep their race number subject to above.

Renewal of race numbers Fee Euro 100.

3 Digit numbers are not permitted.

## SECTION G

### SAFETY RULES

#### GENERAL APPLICATION

The following Safety Rules apply to all X-CAT UIM Series and Championship Events and are in addition to all safety related rules set forth above in the UIM General Racing Rules.

#### 21. REQUIRED SAFETY FACILITIES

In General - All International-Series Event Medical and Rescue facilities must be pre-approved by the UIM at least 30 days in advance of the Event. It shall be the Local Organising Committee's responsibility to communicate with the UIM and perform all safety functions and secure all necessary safety facilities in accordance with these rules and the UIM Safety Procedures and Guidelines Requirements. Each Local Race Promoter shall be required to provide all personnel, materials and services required by the UIM Safety Procedures and Guidelines.

It will be the responsibility of the Local Race Promoter to provide, to complete and return the UIM X-CAT Pre-Race Safety Questionnaire Sheet to the UIM office no later than 30 days prior to the event.

##### 21.01. MEDICAL/RESCUE, TOW AND PATROL BOAT COVERAGE

For detail of Safety/Medical cover requirements, see APPENDIX 1.



## 21.02. SEA SAFETY

Safety Equipment & Sea Safety - Whenever a registered race boat bearing UIM numbers or branding is being operated at a UIM event, including while testing, or at any other times while the craft is on plane, or while off-plane under its own power and within 250 feet of the outside of an active race course, the competitors must wear complete approved and tested personal safety equipment including helmet and life jacket. This rule shall be in effect at a race site from the time of arrival and until one hour after the chequered flag of the last event of the day. Whenever such registered equipment is being operated at times other than UIM events, such operation shall be conducted in a careful and prudent manner according to local rules and regulations with regard to safety, so as not to endanger life or property and so as not to bring powerboat racing into disrepute.

Violation of this rule shall result in a fine or Disqualification from that event or a subsequent event at the discretion of the UIM Race Commissioner.

Pilots must have the full Safety Equipment for all Practice, Pole Position and Races.

It shall be the pilot's sole and ultimate responsibility to ensure the fitness of his boat and to decide whether or not to start or to continue in the race once the boat has passed scrutineering. The Organisers wish to inform competitors that all safety and patrol boats will be instructed that their first priority at any incident is saving human life. If possible, although no guarantee can be given, assistance may be given to boats.

In case of an incident or accident on the racecourse, a yellow flag will be flown or waved by any "Official" boat. Race boats should keep a distance of at least 50 metres away from any boat displaying this flag and the accident. A minimum penalty of a Yellow Card for infringement of this rule will be enforced.

Harbour restrictions. All boats must respect the local rules with reference to the maximum speed in the harbour. A Euro 500 penalty for infringement of this rule may be enforced. Crews may also be subject to Local Authority conditions and possible legal actions.

It is the Team's sole and ultimate responsibility to recover damaged and/or sunken participating race boats.

## 22. FUELLING

Fuelling or decanting of fuel may be made until 24.00 hours (or as announced in the relevant Race Instructions) on the day prior to the race. In the case of Force Majeure, an Official Bulletin may authorise the refuelling on the race day.

Penalty - Disqualification.

Any team which dispenses fuel either in or out of the boat other than in the designated fuelling area, or without prior approval of the Race Director, will be subject to a fine of:

- First time of the season: Penalty - Euro 1,000
- Any future offence: Penalty - Euro 5,000 and/or Disqualification.

Fuelling times will be notified in the Race Instructions and/or Bulletin, posted on the Official Notice Board.

The only fuel approved for use in UIM Events, will be that supplied by the local organiser or series supplier, as the case may be. The Unleaded fuel supplied will have a minimum of 95 Octane. Unless otherwise notified in the Race Instructions.

The addition of 2 stroke oil is permitted.

- No modifications or additives are permitted to the fuel.
- Fuel samples showing any characteristics other than the fuel issued at the event, will be deemed illegal. The penalty is Disqualification from the event for a first infringement, and more serious penalty for a second offence.
- The UIM inspector can at any time take samples from boats to ensure compliance.

Any Team using fuel subsequently deemed illegal for race purposes the team will incur the cost and disqualification.



## 23. SMOKING

“All smoking of any type is prohibited in pit areas”. The deemed area for non-smoking is in the dry pits (excluding inside workshops), craning area and in the wet pits. Competitors are reminded that it is their responsibility to ensure that the rule is strictly adhered to by all their team members. Any contravention of this rule shall incur a penalty of Euro 100.

## 24. RACE OVERALLS AND PERSONAL SAFETY WEAR

Any person aboard any boat partaking in Races, Pole Position and Practice must always wear an adequate race vest as per UIM rules. Life jackets/Buoyancy suits must be worn during all races, Pole Position and practice. Life jackets/Buoyancy suits may only be removed when effecting repairs and with the boat at very slow speed (less than 10 knots). The choice and efficiency of Life jackets/Buoyancy suits is the sole responsibility of the wearer.

Any person aboard any boat partaking in Races, Pole Positions and Practice must always wear an adequate helmet of predominantly orange colour. Helmets must be worn during all races, Pole Position and Practice. Helmets may only be removed when effecting repairs and the boat is at very slow speed (less than 10 knots). The wearer is entirely responsible for the choice and the efficiency of his helmet. Helmets may be removed when returning to pits at less than planning speed.

Each crew member's helmet must be identified with their boat race number. This number shall be placed on the top surface of the helmet and shall be read from the rear. The numbers shall have a minimum height of 7.5 cm and shall be of the same style as the boat race number. These numbers shall be painted in waterproof black paint or be made of black coloured self-adhesive material of adequate strength. The wearer is entirely responsible for the choice of his helmet. The organisers are requested to repeat this important rule in their particular rules, as well as in their Advance Programmes and Race Instructions. Each National Authority may require that their nationals wear a helmet of a type laid down in their own National rules.

At all times during Practice, Pole Position and the Race, competitors or test pilots must wear suitable protective clothing that covers the torso and all limbs to the wrists and ankles. Protective clothing used must be durable enough to provide bodily protection and it is recommended to be fire retardant.

This protective clothing must have the name of the competitor clearly visible on the front chest section or waist band and the competitor must apply to Race Control for a wristband. The inclusion of Blood Group detail added to the race overalls is recommended but not mandatory.

Contravention of any of the above clothing rules will result in a minimum of a Euro 500 fine. It is strongly recommended that neck protection is worn.

## 25. RADIO COMMUNICATION EVENT - PRACTICE / POLE POSITION / RACE

Radio communications with the Race Boats and Team Managers will be maintained on the designated VHF channel as detailed in the Race Instructions and/or Bulletins and/or Briefings.

The Team Manager is responsible for ensuring that all communications to be made to the Race Boats have been received by the pilots on board.

Communications via VHF radio are allowed excluding Official Race Control Channels and Channel 16. Competitors must be fully conversant with, and at all times, must comply with, the International Regulations for the Prevention of Collisions at Sea and such other safety measures promulgated in connection with this event.

Competitors must at all times use the Race Number of their boats their call sign.

Competitors are required to communicate with Race Control before leaving and returning to the Wet Pits at all times. A team representative must be present in the timing area during all on water activity - no boats may leave the Pit area without the presence of the team representative.

No boat will be allowed to leave the wet pit before its radio communications system, as well as that of its Team Manager, has been checked and approved by the Technical Director.

It is mandatory to have at all times during on water activities an efficiently working radio system, and to stay in permanent radio contact with Race Control. PENALTY Euro 500.

It is prohibited to use hand held Radio by the Riding Crew Members to communicate with the race control. Riding Crew members must have a radio capable of monitoring race control integrated in their race helmets. PENALTY – DSQ- Disqualification

Communications - Radio, cell phone or other electronic communication from any aircraft, boat, or shore crew, to any race boat shall be permitted provided that those communications are not used to violate or assist in the violation any of the rules contained herein, and provided further that any team using such communication must provide UIM live access to said communications through assigned radio frequencies or otherwise so that UIM Officials can monitor those communications when requested by UIM. Such communication is prohibited on official Race Control channels.



## SECTION H

### GENERAL REQUIREMENTS, PRACTICES AND PROCEDURES FOR CONDUCTING RACES

#### 26. BOAT PARADE

All boats must take part in the Boat Parade unless previously agreed by the Race Director.

#### 27. TESTING/PRACTICE

Inspections and Registration - Race boats must have a pre-race technical and safety inspection, the boat and all riding crewmembers must complete all registration requirements prior to any on water testing. The owner and all riding crew members must sign the Mandatory Insurance Waiver/Indemnity prior to any on water testing.

Testing must always take place on the race course where possible.

Any boats wishing to practice during the official practice period, must communicate with Race Control on the designated event channel for approval before proceeding out on the course and on safe arrival back in the Wet Pit pontoon, in accordance with the procedures contained in the Race Instructions. Penalty Euro 500.

Practice times will be detailed in the Race Instructions and/or Bulletins. Any practicing outside of these times must be agreed by the Race Director. Boats must not cut across the practice course except in the case of Force Majeure and with extreme caution. Penalty for non-compliance - Euro 500 per boat for first offence, Euro 1,000 for subsequent offences in the event and/or the Series/Championship.

Official Practice will only take place after Administration and Scrutineering have been completed satisfactorily.

Penalty for non-compliance will result in a penalty of Euro 1,000.

#### 28. POLE POSITION

A Pole Position will be held on the race course either by designated Pole Position session the day before the race, the final practice session unless otherwise stated in the Race Instructions or posted by Race Bulletin as change due to unforeseen local conditions or weather.

The UIM will approve all rules, procedures and guidelines for the official Pole Position.

All communication will be on the event official VHF channel, as announced in the Race Instructions, via the Team Manager and/or Race Boat.

Boats may run in accordance with the official "On-Water Procedures and Guidelines". As per Race Instructions.

#### PROCEDURE - APPENDIX 2-01.

#### 29. RACE COURSE

##### 29.01. RACE COURSE LENGTH AND RACE COURSE DESIGN

The UIM must approve all X-CAT Series Race Courses. Courses must be submitted by the Local Organisers at least 30 days prior to event.

The promoter and/or the UIM will correspond with the drivers representative with the draft race course for comments and observations in writing for consideration by the UIM and within 48 hours of the notification to the representative.

##### 29.02. RACE LENGTH

The X-CAT Race lap must be a minimum of 2.8 Nautical Miles and a maximum of 4 Nautical Miles. Race Laps must be repeated. The minimum distance for race duration is 60 Nautical Miles or 1 Hour plus a lap, or as stated in Race Instructions.

Under no circumstance during the event may any boats practice, Pole Position or race outside of the designated area. Failure to comply will result in a fine of up to Euro 1,000.

Failure to complete the course correctly as described in the Race Instructions and/or at Race Briefing and/or Race Bulletins, will result in time added or lap penalties.





### 29.03. LONG LAP

It is mandatory for all racing boats to complete the Long Lap(s), as specified in the Race Instructions, at any time from the start of the third lap; the first long lap is to be completed before the end of the 50% lap and the second by the end of the 70% lap. Failure to complete the Long Laps will result in time added or lap penalties, as follows:

- Failure to do the first long lap by the end of the 50% lap - Penalty 1 Long Lap.
- Failure to do either of the 2 long laps - Penalty Disqualification.
- It is recommended that the Long lap should be approximately 25 and 35 seconds longer than the course race lap.

### 29.04. BAD WEATHER

If it is necessary to run a Bad Weather course and/or bring the Race start time forward, full points will be given provided:

- The length must be a minimum of 50% of the original programmed number of laps.
- A minimum of one-hour notification must be given prior to the start time.
- The Teams' Representative, Race Director, Promoter's Representative, Safety Coordinator and Local Race Officer must agree the bad weather course and the number of laps.

### 29.05. START CHUTE

Minimum Width - The recommended minimum starting chute width shall be 30 metres per race boat.

Minimum Length - The recommended minimum starting chute length shall be 1.5 miles before the first turn mark.

If the starting chute length is less than 1,5 nautical miles, the first turn must not be more than 90 degrees to the second buoy of the turn.

Other Requirements - The chute must be maintained in a straight line so that the boats can maintain their position. The start cannot be set up with a turn before the end of the chute.

The start run must allow for the pole position boat to have the shortest distance to the first turn mark.

The start chute may be clearly marked, buoys or fixed points on either side of the course. All competing boats must pass through the start chute between the markers.

The finish boat shall fly a X-CAT Flag or a specifically-designated flag, and chequered flagging will be done from the official finish boat.

### 29.06. TURN MARKS

All turn marks on the course, approved by the UIM, will be published in the Race Instructions.

When a turn is less than 90 degrees, it should be designated by a minimum of 3 mark buoys a minimum of 100 metres apart.

Above 90 degree turns should be designated by a minimum of 2 mark buoys a minimum of 100 metres apart.

Any buoy used to designate race marks should be a minimum of 1.5 metres tall, and made of vinyl or plastic material capable of withstanding conditions encountered in powerboat racing.

It is recommended that where turn marks are located in the vicinity of permanent navigation marks, the turn mark must be laid to the outside of the permanent mark at a minimum distance of 10 metres.

Each turning point must be marked by a buoy in accordance with the relevant guidelines and controlled by the Course Officials. Different colours must be used to indicate whether the boat must pass inside or outside the buoy.

### 29.07. TURN MARKS AND/OR ABSENT OR NON-CONFORMING TURN BOAT OR BUOY

The procedures for passing buoys are included in the UIM On-water Procedures and Guidelines (Appendix 2), with related illustrations of the various scenarios in the turn Buoys Clarification Drawings (Appendix 4)

All marks are laid in the approximate positions referred to in the Race Instructions and/or Drivers' Briefing and/or Race Bulletins.

It is the competitor's responsibility to drive close enough to the course marks to ensure that he/she has been seen to pass correctly and to satisfy him/herself to this effect.



Unless otherwise specified in the Race Instructions and/or Drivers' Briefing and/or Race Bulletins, the colour of the Course Buoys must be as follows:

All Outside Course Marks YELLOW

All Inside Course Marks ORANGE

Should a buoy go 'missing', the pilot must pass through the approximate geographical position referred to in the Race Instructions and/or Drivers' Briefing and/or Race Bulletins. Penalty for not passing through the geographical position or around the Boat waving the yellow flag - one Long Lap or Time Penalty 30 Sec minimum, 40 Sec Maximum.

If a boat hits a buoy on the correct side, no action will be taken. If a boat goes over a buoy, demolishes and/or dislodges a buoy, a one Long Lap or Time Penalty will be applied.

If the buoy is also demolished, the team will be fined Euro 1,500 to refund the cost of the buoy. See (Appendix 4)

The repetition of missed buoy is not allowed – Penalty: DSQ and Yellow Card

Any buoy passed on the wrong side during practice may result in a Euro 500 fine, and if safety or other boats are compromised a Yellow Card.

Course IN and OUT Procedures and mark buoys must be follows at all time during Practice, Pole Position and Race.

Failure will result in Euro 500 fine.

Infraction during race will result in additional time penalties and/or disqualification.

Any buoy passed on the wrong side will result in a Penalty specified in the table bellow:

(See also Appendix 4).

<b>Turn Mark Comprising of 1 buoys</b>		
1 Buoy missed	=	DISQUALIFICATION
<b>Turn Mark Comprising of 2 buoys</b>		
1 Buoy missed	=	One Long Lap Penalty or Time Penalty
2 Buoys missed	=	DISQUALIFICATION
<b>Turn Mark Comprising of 3 buoys</b>		
1 Buoy missed	=	One Long Lap Penalty
2 Buoys missed	=	Two Long Laps Penalty or Time Penalty
3 Buoys missed	=	DISQUALIFICATION
<b>Repetition</b>		
Repetition of a missed buoy	=	DISQUALIFICATION
<b>Course Correction</b>		
Re-taking of a missed buoy or turn is not permitted	=	DISQUALIFICATION

#### 29.08. RIGHT OF WAY

Overtaking - When two race boats are on the same course or approximately the same course on a straight, and one boat is passing or attempting to pass another boat, the passing boat shall keep clear of the boat being passed, and in passing, shall allow at least 2 boat-lengths of clear water between its transom and the bow of the boat being passed, before altering its helm so as to assume the same or essentially the same line or lane as that of the overtaken boat.

Passing in a Turn (Formerly "Overlap") - If two or more boats are approaching a turn mark side by side, the boat on the inside of the turn has priority and the outer boat must keep clear.

Any overtaking boat must still give way to the overtaken boat as per overtaking above.

Priority must be given to the race boats on the race lap.

Long Lap - Boats returning from their long lap to the race lap must give priority and give way to the boats on the race lap unless they are clearly ahead as overtaking above. The return to race lap line is at any point when returning from Long Lap mark buoys and rejoining the race line at any point.

Yellow Flag - On seeing the Yellow flag signal from a safety boat, race boats must slow down, acknowledge the signal, proceed with caution and keep clear of the danger area. No overtaking is permitted under this caution. Any competitor who ignores the Yellow flag signal will be penalized and/or issued with a Yellow card and minimum Euro 1000 Fine.

Red Flag - All boats must safely slow from racing and make their way under safe speed as described in Race instructions or drivers briefing to the muster/milling area.

#### 29.09. SPECTATOR AREAS

Organisers should designate spectator areas. If a race boat finds itself in a spectator area, it must reduce speed to less than 12 knots immediately and proceed at less than 12 knots with caution until back on the race course.

Penalty Yellow Card.

#### 30. LAUNCHING

Launching and recovery facilities are available to competitors during Official Races, Pole Positions and Practice. The operational hours are indicated in the Race Timetable. No facilities will be available to competitors until they have signed in and completed Administration and Technical Inspection.

All boats' movements, launching and recovery instructions will be under the responsibility of the Technical Director. Once signed in, no boat may leave the compound for any reason unless agreed by the Race Director.

At the end of the Pole Position and the Grand Prix race, unless otherwise advised by the Technical Director, boats are obliged to return to the crane from which they were launched.

In all cases when a crane is used, it is forbidden for any person to be on or in the race boat while the boat is being lifted. The penalty for non-compliance will be a Euro 1,000 penalty and/or Disqualification from the event. Exception is rule 32.02 Pit Stops.

The order of launching for the race and the choice of the crane and launch time will be decided by the classification order of the Pole Position, or based on the previous race result if no Pole Position takes place.

If the race boat is not ready to be launched at its designated time:

- The team may be fined Euro 500
- The boat will be launched at a time to be decided solely by the Technical Director.

No boat will be recovered from the water once launching has commenced unless with the permission of the Technical Director.

If a race boat, having been launched needs to use the crane, it may only do so after all race boats have been launched and with the permission of the Technical Director.

No propeller or ratio changes will be permitted until the start has taken place and the change will be deemed a pit stop.

#### 31. CRANING

Craning time is fixed at a maximum of ten minutes. A boat can enter the craning area only when it is ready to be hoisted. The time starts from when the boat is attached to the crane by its straps and ends when the straps are released from the crane.

Boats exceeding the craning time during pole position or race will not be permitted to continue in the Pole Position or race.

Practice session time infringement Penalty Euro 500.



## 32. PIT STOPS

### 32.01. DEFINITIONS

Pit Stops - visit to the designated Pit Stop area for assistance during the Race and/or Pole Position i.e. from the time of the race Start until the time the last boat crosses the finish line. The Pit Stop area is the Wet Pit unless otherwise defined in the Race Instructions.

### 32.02. PIT STOP RULES

The Pit Stop procedure is under the responsibility of the Technical Director.

No-one is permitted in the water during pit stops.

Whilst on the crane pilots may remain harnessed for prop changes (safety issue exiting and entering the boat). In this case, 1 x support crew to check with race crew that BATTERY SWITCHES ARE OFF for duration of change. No Crew are allowed on deck Penalty Euro 1000. If advantage gained and/or time penalty and/or disqualification.

PROCEDURE - APPENDIX 2-02

## 33. OUTSIDE ASSISTANCE

Refuelling is not authorised for any race boat whilst competing in the Pole Position or Grand Prix race.

Outside assistance may only be provided in the officially designated Pit Stop Area.

Changes - No change is allowed during Pole Position and Grand Prix Race, except if conforming to Pit Stop Rules. Otherwise the Penalty is Disqualification.

The Race Director has the right to decide if there was illegal outside assistance, which will result in Disqualification.

## 34. START PROCEDURE

Start/Pace Boat - The Start/Pace Boat Procedure is as described in the latest version of the Appendix 2 UIM On-water Procedures and Guidelines and/or the Race Instructions or race bulletins and must be followed at all times.

Description - The description of the Start/Pace Boat will be given in the Race Instructions.

Safety Equipment:

- Life Jackets to be worn by all onboard personnel.
- Radio VHF marine system.

Start Procedure - The Start Procedure is described in full in APPENDIX 2-03.

## 35. FINISHING PROCEDURE

### 35.01. STOPPING THE RACE

Emergency Race Stoppage Procedures/Restarts (Force Majeure).

The procedure is included in APPENDIX 2-04.

Only the Race Director shall have the authority to stop an event.

Red Flag race stopped procedure (see Pace Boat Procedure Appendix 2-04).

The severity of the incident and race stop decision is taken only by the Race Director with information from his race officials.

If the race is stopped after completion of 70% of the race laps, then the boats will be classified based upon their position when crossing the previous lap timing line. In this case, a restart will not be implemented.

### 35.02. CURTAILING THE RACE

In the case of Force Majeure or accident, the Race Director may curtail (shorten) the race by the waving the Finishing (Chequered) Flag. Boats will be classified on completion of their current lap, based on their position at the Finishing line.

### 35.03. STOPPED / CURTAILED RACE

In the event of a Stopped or Curtailed Race, if the 'winning boat' has completed:

- A minimum of 50 % of the actual programmed number of laps: full points will be awarded.
- Between 25% but less than 50 % of the actual programmed number of laps: half points will be awarded.
- Less than 1 lap: the race may be restarted using the normal procedures.

Any Engine Bonus points are awarded following a race start if stopped by this procedure.

In the event of a Stopped or Curtailed race, boats must have completed a minimum of 70% of the laps completed by the 'winning boat' to be classified a finisher.

#### LONG LAP CALCULATION "Stopped/Pace boat Procedure"

- If a race is stopped before the completion of 70% and the Pace Boat is used, the mandatory long laps can be done after the restart at any time before the end of the race.
- If the race finishes while the Pace Boat is running 1 hour (time Limit), the order of the boats will be used for the final classification. Time compensation will apply provided that all the boats have done at least one Long Lap. In this case, the boats that have done a Long Lap will be compensated replacing their long lap time with the time of their best race lap (excluding the first/start lap).
- If the race finishes by the 50% lap, one Long Lap will be counted.
- If the race finishes by the 70% Lap, then two Long Laps will be counted.

#### LONG LAP CALCULATION "Stopped/Curtailed"

- If the race is curtailed between the beginning of lap 2 and the 50% lap and not all the boats have done at least one mandatory Long Lap, none of the long laps will be counted. The boats that have done a Long Lap will be compensated with the time of their best race lap (excluding the first lap/start lap).
- If the race is curtailed between the 50% and the 70% laps, two long laps will be counted. Time compensation will be applied to the boats that have done only one mandatory Long Lap, replacing their fastest Race Lap with the time of their previous mandatory Long Lap.

### 35.04. FINISHING PROCEDURE

White Flag - When the leading boat crosses the finish/timing line to commence its last lap, a white flag with X-CAT logo will be raised on the Finish Boat.

The white flag will also be announced via radio and timing computer.

Chequered Flag - When the leading boat completes the course and crosses the finishing line, the chequered flag will be waved. It designates the 'winning boat' for the Race and the start of the finish procedure.

The chequered flag will also be announced via timing computer.

The full finishing procedure is included in APPENDIX 2-05.

- The Winner, 2nd and 3rd place will be awarded for the Grand Prix.
- In order to be classified as an official finisher, a race boat must make a legal start. The Driver and Throttleman must remain in the boat during the entire race, except during Pit Stop procedure.
- After crossing the finish line, a race boat shall not interfere with any other boat still in the race so as to affect the time of such boat at the finish or endanger its crew.
- Official race results shall be posted on the Official Notice Board and on the UIM website and X-Cat website.

A boat is timed when the bow crosses the finishing line.

A boat that finishes a race must follow the procedure described in the UIM On-Water Procedures and Guidelines and Race Instructions and/or



Bulletins and/or Pilots' Briefing. Penalty Euro 1000 and Yellow Card.

The Team support crew cannot under any circumstance open any engine, cockpit or any other hatch on the race boat until they receive the permission of the Technical Director. Penalty Disqualification.

Posting of Results - The arrival order will be posted at a designated location, and may be announced on radio after the race. A racer or team cannot badger any officials for their decision which they are ruling on.

For any race that is subject to Post-Race Dyno Testing, the finish order is provisional pending report from Dyno Test Facility.

As soon as the results have been officially confirmed by the Race Director the final official results are posted. Once the official posting of the results has taken place, there is a period of one hour in duration during which protests can be made. These results are deemed to be provisional for the purposes of post race dyno and/or engine inspections testing and subject to any video evidence of race infractions.

#### 35.05. 70% RULE

##### CALCULATIONS (PERCENTAGE/FRACTION)

Throughout the % rules, where a percentage/fraction needs to produce a whole number, the number will be rounded up or down to the nearest whole number. Where the percentage/fraction results in .5 (1/2) or less, the number will be rounded down unless specifically stated in the rule.

For example:

- 70 % of 7 laps is 4.9, 5 laps would count.
- 70 % of 5 laps is 3.5, 3 laps would count.

The minimum number of laps to qualify as a finisher may also be announced at the relevant Pilots' Briefing and/or bulletin. This should equate to 70% of the laps of the race.

Any boat crossing the finishing line after the 'winning boat', will be classified based on the number of laps completed and its time, provided it has covered the minimum number of laps.

Any boat that fails to cross the finishing line after the winning boat will be classified based on the number of laps completed and its time, provided it has covered the minimum number of laps.

The Course will close 15 minutes after the 'winning boat' has finished unless otherwise stated in the Race Instructions, Race Briefing and/or Bulletin. Any Lap completed after this time will not be included in the final results.

#### 35.06. TIME LIMIT

All Grand Prix races will have a maximum time limit of 1 hour. The chequered flag will be waved when the lead boat crosses the line after the 1 hour maximum time limit, and all boats will finish as they complete that lap. The number of laps completed by the 'winning boat' will determine the number of laps for the 70% rule. Alternate distance may be set by Race Instruction. Extreme Cat have maximum time limit of 60 mins + 1 lap unless stated otherwise in race instructions.

#### 35.07. NO FINISHERS

Should no boats finish, each boat will be classified based on the number of laps completed and its time, provided it has covered the minimum number of laps of the 70% rule, as announced at the relevant Pilots' Briefing and/or Bulletin.

#### 35.08. RETURNING TO THE PITS

When a race boat retires, the strobe light must be on until in the wet pit. A Euro 500 penalty for infringement of this rule may be enforced.

#### 35.09. RETIREMENTS

Boats, when retired, must report to Race Control on the VHF channel detailed in the Race Instructions. If a boat has announced its retirement to Race Control, the timing will be stopped at the moment of the announcement. Retired boats are forbidden to proceed on the course and must return to the wet pits, with assistance if necessary.



### 35.10. POST RACE

After the end of the race, to be classified as a finisher competitors must not receive any outside assistance, go alongside, collect other persons aboard, allow access to any deck/engine hatches or remove or place in electronic equipment etc., before reporting to the official Post-Race Scrutineering area and being cleared by the Technical Director. Disregard for this rule will result in Disqualification. With the exception of medical or mechanical emergency by permission of Race Control.

## 36. POST RACE TECHNICAL INSPECTIONS

### 36.01. ALL BOATS SUBJECT TO

All boats are subject to a post race technical inspection conducted by a Technical Inspector. A list of approved inspectors shall be issued annually by the UIM O.P.C.

### 36.02. UIM TECHNICAL DIRECTOR DISCRETION

The Technical Director has the discretion to develop and implement a Technical Inspection Plan for each Event, and may vary the items and boats to be inspected at each Event. The Race Director may also direct the Technical Director to conduct an inspection of any competing race boat.

### 36.03. FINAL RESULTS

No points shall be awarded until the legality is confirmed by the Technical Director to the Race Director, but still remain provisional subject to post race dyno and/or engine inspection and any video or virtual/Telemetry evidence regarding race infringements.

### 36.04. ENGINE INSPECTIONS

At the discretion of the Race Director or Technical Director, any engine may be stripped down to check legality. It is also the Race Director's and Technical Director's right to request re-inspection at any point up to the presentation of awards. When an owner or pilot has been notified that his equipment is to be stripped down, he has the option of having either an "Open" inspection (in full view of competitors and any other observers) or a "Closed" inspection (open only to the official Technical Director's and the Race Director assigned to this inspection). The results of a technical inspection may be made public by a designated UIM Official but only after the information has been transmitted to the Team Owner.

Engines may be sealed for transport to official inspection or dyno testing facility.

### 36.05. REFUSAL OF INSPECTION

Any entrant to UIM events refusing an inspection after having been notified by the Race Director or Technical Director that equipment within his control must be checked for conformity with safety and/or technical specifications shall be disqualified from that event and suspended for sixty (60) days.

Violations - In addition to the penalties provided in above clauses of Rule 36, the following procedures and penalties shall apply to post race technical inspections and violations.

First Offence - A first offence of a technical, weight, safety nature at any time during the annual registration of the boat in question may result in the forfeit of monies for the event, a deduction in position for that event, Fine Euro 1,000 - 10,000.

This is applicable unless a penalty is established in any other specific rule.

Second Offence - A second offence of the same nature at any time during the annual racing registration of the boat in question shall result in a mandatory Disqualification from the event.

Subsequent Offence(s) - A subsequent offence(s) of the same nature at any time during the annual racing registration of the boat in question will result in mandatory Disqualification from the event and the current racing year.

Team Responsibility - The owner shall be responsible for the condition of the engine and hull as raced. Errors on the part of the manufacturer, boat builder, engine builder, mechanic or previous owner shall not excuse non-compliance with the rules. The Owner is also responsible for their team's conduct at an event.

Additional Responsible Parties - The party responsible for making illegal modifications to engines shall be subject to up to one year suspension from UIM events and /or a Euro 50,000 fine and if a commercial homologated vendor, possible barring of all equipment prepared by them for competition in UIM Events.

### 37. POSTPONEMENTS

If a race cannot be run due to inclement weather or any other unforeseen circumstances, it can be rescheduled to run on the day following the scheduled, date or a storm course may be substituted. This decision shall be made by the Race Committee in conjunction with the Pilots' Representative and Local Authorities.

Hourly postponements will be permitted when weather or other conditions are such as to make it unsafe to start a race at its designated time, but there is a reasonable chance that conditions will improve. In no case shall a race be started if it will place race boats on the course within one (1) hour before sunset.

If a race cannot be run due to inclement weather or any other unforeseen circumstances, and cannot be rescheduled, the entry fee is not refunded.

Changes to the Event - Any race postponement or substitution of an alternate storm course shall be announced by Race Bulletin. The Race Committee shall adhere to the schedule set forth in the Race Instructions unless some emergency shall dictate otherwise.

In the case of any such changes, the UIM Race Officials must notify the teams at least one hour prior to the race.

### 38. PRIZE GIVING

1<sup>st</sup>, 2<sup>nd</sup> and 3<sup>rd</sup> place will be awarded for the Grand Prix.

Additional prizes may be awarded at the discretion of the Organisers.

The No.1 and No.2 pilots of the first three classified boats must appear at the prize-giving (and at the following press conference if any) at each X-CAT Heat, unless agreed in writing by the X-CAT Promoter. The prize-giving (and the press conference if any) will be held within one hour after the end of the race. The pilots must wear their own race overalls.

Penalty for either of the above: Euro 1000 fine.

### 39. LAY DAYS

Lay days are allowed if previously agreed with the UIM and announced in the Advance Notice.

### 40. RESCHEDULING OF RACES

Should it be impossible to hold a scheduled race due to Force Majeure, the UIM may reschedule to race at a different time and place, the commitment of the teams to participate remaining unaffected, provided a minimum of 28 days' advance notice is given prior to the date of the original event that is being cancelled. Unless agreed by the promoter and the UIM.

If insufficient notice is given, the race may not be rescheduled and may be lost.

Reserve dates/venues will be fixed by the promoter in agreement with UIM.





# PART 4

## TECHNICAL RULES

### X-CAT - TECHNICAL SPECIFIC RULE

01. HULL MEASUREMENT
02. HULLS MINIMUM/MAXIMUM LENGTHS AND WEIGHTS
03. MINIMUM FLOTATION
04. HULL/DECK REQUIREMENTS
05. MANDATORY EQUIPMENT
06. ENGINES
07. GENERAL ENGINE MODIFICATION RULES
  - 07.01. NON EPA/CARB PERMITTED MODIFICATIONS
  - 07.02. MODIFICATIONS NOT PERMITTED (EPA/CARB)  
OUTBOARD ENGINES
08. GEARBOX & WATER PICK UP
09. PROPELLORS
10. SCORING SYSTEM
  - 10.01. 21-25 BOATS: 5 BONUS POINTS
  - 10.02. 26-30 BOATS: 10 BONUS POINTS
11. CANOPY RULES & CREW SAFETY
12. DRAWINGS AND MEASUREMENT
13. REINFORCED COCKPIT AREA AND CANOPY
14. OTHER REQUIREMENTS

## X-CAT – TECHNICAL SPECIFIC RULE

### Hull Dimensions Requirements etc.

The UIM Extreme Cat “X-Cat” class strictly prohibits the use of mono hulls and inboard engines.

WPPA X-Cat Class racing shall only permit the use of boats powered by Outboard engines.

Implementation will be by notice from UIM to all teams.

All equipment fitted to the race boat or used for the purpose of X-Cat racing must be available commercially, Any equipment or component not covered by the rules must be approved by the UIM before its use in X-Cat

It is forbidden to use any form of live telemetry, Data loggers may be used but can only be accessed post race or Pole

### 01. HULL MEASUREMENT

All hull measurements are to be taken while the boat is ashore.

The length must be measured between perpendiculars of the external molded length of the boat, taken at the extreme bow and the rear most planning surface (the rear most part of the keel) including all hull planning steps (regardless of height), but excluding trim tabs, either fixed or movable.

Hull extensions beyond the transom shall be deemed to be “fixed rim tabs” and shall not be included in the measured length.

Hull extensions added to the bow and/or any extending parts, rubbing strakes, fenders, outboard motor spacing brackets, stabilising trim tabs and rudder assemblies are not to be included in the measured length.

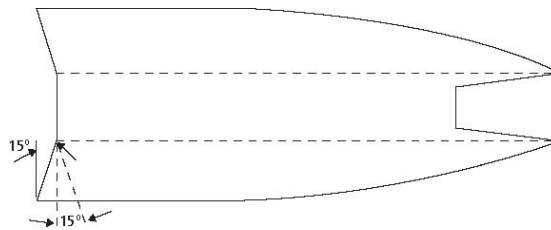
When defining “extreme bow”, any form that constitutes the bow and its construction and contributes to the performance of the boat shall be included in the measured length. Any other attachment that is added to meet a minimum length requirement shall not be included in the measured length.

Should the trailing edge of the planning surface of a hull or sponson terminate with an angle greater than 15° measured between the keel and outer chine in plan view (birds eye view), such excess shall not be included in the measured length.

The beam measurement shall be taken at the widest part of the moulded hull, excluding any extending parts, rubbing strakes and/or fenders.

Trim tabs, either fixed or moveable, shall not exceed a length of 10% of the measured length of the hull.

Tunnel flaps shall not exceed a length of 10% of the measured length of the hull.



NOT GREATER HAN 15°

CATAMARAN IN PLAN VIEW

## 02. HULLS MINIMUM/MAXIMUM LENGTHS AND WEIGHTS

It is mandatory that fully dimensioned drawings of all liquid containers and or tanks are given by each team before the first race to the UIM Technical Director. This drawing shall include measurements from the nearest reference point e.g bulkhead to show its location . Methods of filling and emptying must also be shown and the tank must be able to be checked if empty or not all onboard liquid ballasting must be coloured with permanent dye.

Liquid Ballasting is not permitted to be pumped in from the sea or pumped overboard during the Race or Pole position.

The UIM X- Cat class weight criteria for both Non EPA and EPA engines, is as follows:

Non EPA	EPA
4101 - up to 6000cc	up to Max 2 x 300hp

Max HP — 600

All EPA engines must be approved by the UIM Technical Director for eligibility if new to the X-Cat series.

All weights and dimensions below refer to the use of both EPA and non EPA engines.

The weighting procedure will be defined as the scale of the day (further instructions will be published in the Race Instructions).

Min Length 7.6m  
Max length 10.00m

Min kg 1755kg

Add kg per meter 110kg for boats longer than 7.6 meters

Cube

- Length 3.60m  
- Width 1.20m 1.20m  
- Height 0.46m

Bouyancy

- GRP 550l  
- Comp 300l

The minimum weight mentioned is for a boat with the mentioned minimum length. For boats longer than the minimum, the weight shall be increased according to the formula shown above.

Minimum weight is calculated as post race, including all safety equipment (except the crew and their helmets and life vests), residual fuel in tanks will be included in the minimum weight. Slings carried in the boat during the race will count as safety equipment. The boat must be empty of water.

## 03. MINIMUM FLOTATION

All boats shall contain a minimum volume of buoyancy permanently installed in the hull.

The minimum volumes measured in litres are mentioned in the chart.

If a boat is constructed of wood, the wooden weight content of the boat may be accepted as buoyancy as per the following formula : Wood weight in kg x 2 = litres of buoyancy.

## 04. HULL/DECK REQUIREMENTS

All hulls racing must have side by side seating canopies with secondary escape hatches. Tandem and/or separate sponson type canopies are not permitted.

The deck must be able to bear the weight of a standing man (100 kg) at any point.

All cable and wiring deck exits should be watertight.

All boats shall be fitted with a well secured mooring cleat or tow bracket adequate for anchoring in a seaway and for towing at sea over a prolonged period.

The hook is to be fitted on a reinforced area designed for the purpose of towing a water laden hull.



Boats must be fitted with a minimum of three lifting eyes, either through deck or topside hull cleat type, suitably engineered to withstand lifting the boat fully equipped and fuelled with an adequate safety margin.

Each boat shall be equipped with a detachable strop assembly providing a single lifting point.

Boats must have fuel filling and fuelventing capabilities outside of the crew containment area.

Canopied craft are to have fuel tanks contained in sealed compartments from the hull and crew area to avoid leakage of liquid or vapour being released into the area being used by the crew. Fuel filling / fillers are to be outside of the crew compartments.

The fuel tank or tanks must be grounded to enable the discharge of static electricity.

Fuel tanks may be constructed as an integral component of the hull construction, or may be fabricated material suitably secured to the hull.

Purpose designed and constructed rubberised collapsible fuel cells may be used.

It is recommended that new boats are fitted with explosafe type fuel tanks.

#### 05. MANDATORY EQUIPMENT

Any television equipment and/or telemetry equipment required by the UIM or its contractors or associated equipment and is deemed to be mandatory and must remain in place at all times and maintained for the intended purpose.

When additional electric pumps are fitted to the fuel supply a fuel cut-off switch for the pump shall be fitted within reach of either crew member.

It is recommended that all crews carry emergency harness cutters as used by emergency rescue services.

On board air systems are currently under review by the UIM. It is recommended that all crew carry secondary/spare systems for use in emergency evacuation.

Other items may be required as listed on the class scrutineering checklist.

#### 06. ENGINES

To be eligible for racing, teams must submit two brand new Ecus. The Ecus will be held by The Technical Director. Teams will be given two random ECUs upon registration to race; NO other ECU may be used for the race weekend. Non conformance, interruption or modification of the Ecu or signal will result in a minimum of disqualification. The race organisers/committee cannot be held responsible for any failure or defective Ecu or connections, causing interruption or loss of position to the race or racers

The Technical Director may at anytime request the engine ECU,s for recalibration or inspection. The recalibration may include a reduction in RPM or total power output. Any recalibration to reduce either RPM or power output on the grounds of safety may take place for all boats at any designated time in the race year or end.

Existing Extreme Cat engine rules in place in the GCC Championship series are eligible.

Manufacturers new models EPA/CARB type engines may be used subject to the UIM O.P.C approval. Only those that closely match exiting horse power capabilities will be considered. No modifications will be permitted.

All engines must conform to those entered and accepted on the UIM measurement certificate.

Internal or external spraying of any substance into the airstream except fuel through the engines injection system is forbidden.

#### 07. GENERAL ENGINE MODIFICATION RULES

Revolution counters, tachometers, water pressure and temperature gauges may be fitted to the engines.

Thermostats of the cooling circuit may be removed.

The use of an ECU outside of the official homologated standard offshore equipment or factory offshore standard, or any other device is not permitted. The modification and/or reprogramming is strictly forbidden.

Re-boring and fitting of oversize pistons is allowed as made available by the manufacturer for that model.

The steering bar and brackets may be reinforced as a safety measure.

It is also permitted to modify the standard engine cowling in order to facilitate the fitting of steering assemblies, provided that any openings are sealed so the engine can not pull in any additional air.

The rubber mounts of the engine may be changed or substituted.

A spring may be added to the intake butterfly valve.



All external (not contained within the powerhead) studs, screws, nuts, bolts and their washers are free as well as the method of locking them, providing there is no performance advantage.

Manufacturers available counter rotating gearboxes may be used.

The use of thrust blocks mounted on the lower unit and or on the boat is permitted.

Power Trim: a properly engineered system for altering engine trim whilst under way is permitted.

Transom brackets may be reinforced.

Lighter flywheels as made available by the manufacturer may be fitted as per homologation tolerances.

#### 07.01. NON EPA/CARB PERMITTED MODIFICATIONS

1. All measurements are to be taken from the UIM Homologation file 00491 for the Mercury 2.5 EFI offshore race engine.
2. All other components of the 2.5 EFI outboard engine not given a dimension or mentioned in the Homologation file or X-CAT Rules, must remain standard (outboard engine is defined as all components from gearbox to top engine cover).
3. The motor type, and visual appearance must conform to the homologation file.
4. Measurements not quoted in the homologation file may not be altered. If a detail cannot be verified with the aid of the pictures in the homologation file, the part in question can be compared to a corresponding stock part. In such a comparison it is only possible to approximate the comparable measurements as the manufacturing tolerances are not available.
5. A component may be machined in order to reach a specific dimension, which is quoted in the engine homologation file. A component for which the weight is quoted in the homologation file may be adjusted down to the weight, by machining or grinding provided that the area where metal is removed is the same as is used by the engine manufacturer. This is known to be at the top or at the bottom, or both for piston connecting rods. Balancing of one or all piston connecting rods is permitted, as long as this is in accordance with manufacturer's procedures.
6. Polishing of connecting rods is not permitted.
7. When the volume of the combustion chamber is specified in the homologation file, the cylinder head may be faced to achieve the volume allowed.
8. When ports in cylinders are adjusted to the dimensions specified in the homologation file, material may only be removed in the specified opening to a depth of 10 mm, to match the shape of the original adjacent connecting passage (channel) outside the adjusted port opening. The dimension is for blending of the port opening to the port passage only
9. The measurement for the cylinder wall is from the inside of the liner where the piston runs to a total 10mm depth or length after grinding/blueprinting.
10. The Blueprinting/grinding dimension gives no allowance to reshape or deepen a port that is not 10mm deep to a depth of 10mm (original depth must be maintained).
11. The 10 mm blending rule applies to all openings given a dimension and tolerance on the homologation file.
12. It is permitted to replace lubricating systems using oil tanks and oil pumps with lubrication oil mixed into the fuel. Caps or covers in the motor cover cannot be removed to provide extra air inlet.
13. The fuel connector in the lower cover may be removed and the fuel hose from the fuel tank connected directly to the fuel pump. If this is done, the opening left after the connector and around the fuel hose must be sealed to prevent extra air entering inside of cover.
14. Fuel flow regulator may be substituted. The original manufacturers fuel flow pressure must be in place and unchanged.
15. Mercury 2.5 EFI the exhaust plate may be removed.
16. Fuel must not be chilled or cooled in any way.

07.02. MODIFICATIONS NOT PERMITTED (EPA/CARB)  
OUTBOARD ENGINES

The following modifications are expressly NOT permitted.

- Blueprinting' of the powerhead.
- Alteration or modification of any powerhead components.
- Removal of material from the powerhead for any reason other than recognised (by the manufacturer) repair.

08. GEARBOX & WATER PICK UP

The water intake may be repositioned; it must however remain on the gear case torpedo. The width or height of the gear case torpedo must not be changed. The length fore and aft may be increased. The rear edge of the water intake opening must not be further back than 30% of the total length of the modified gear case torpedo.

The original intake to the circulation pump may be filled in and faired.

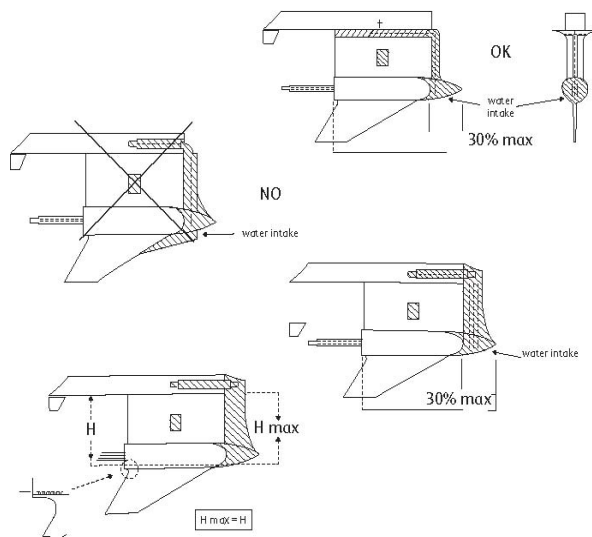
For cooling purposes it is allowed to add material in front of the union leg, as long as the union leg width is not increased, except where the union leg meets the lower side of the anti-cavitation plate.

The modification shall be for cooling purposes only and shall form a radius. The radius shall be lengthwise parallel to the anti-cavitation plate. It is also permitted to add material to the centre section exhaust housing for the purpose of reducing noise.

Remote water pick-ups are not allowed.

Bearing carriers and propshafts may be changed from original.

All craft must be able to be maneuvered ahead and astern and have neutral capability, operated by controls at the helmsman's position. This facility must be a permanent installation and able to be demonstrated as practical at any time.



GEARBOX DRAWINGS

## 09. PROPELLORS

The use of forged propellers is strictly prohibited. Maximum number of permitted blades is 5.

## 10. SCORING SYSTEM

Unless otherwise specified, the following scoring system is to be adopted:

1 - 400 pts	11 - 22 pts
2 - 300 pts	12 - 17 pts
3 - 225 pts	13 - 13 pts
4 - 169 pts	14 - 9 pts
5 - 127 pts	15 - 7 pts
6 - 95 pts	16 - 5 pts
7 - 71 pts	17 - 4 pts
8 - 53 pts	18 - 3 pts
9 - 40 pts	19 - 2 pts
10 - 30 pts	20 - 1 pts

For events with more than 20 entries, the following bonus points should be given, so that all drivers can get points.

### 10.01. 21-25 BOATS: 5 BONUS POINTS

Position 1) to 20) : should all have 5 extra bonus points

Position 21 : 5 points

Position 22 : 4 points

Position 23 : 3 points

Position 24 : 2 points

Position 25 : 1 point

### 10.02. 26-30 BOATS: 10 BONUS POINTS

Position 1) to 20) : should all have 10 extra bonus points

Position 21 : 10 points

Position 22 : 9 points

Position 23 : 8 points

Position 24 : 7 points

Position 25 : 6 points

Position 26 : 5 points

Position 27 : 4 points

Position 28 : 3 points

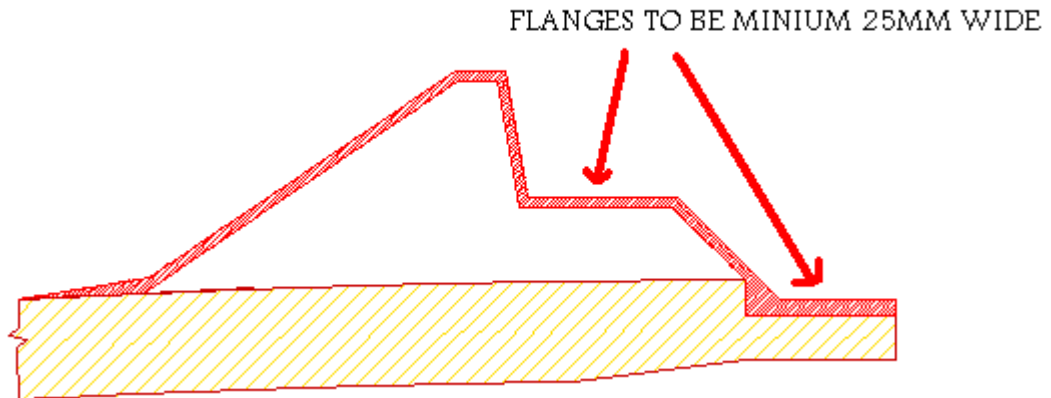
Position 29 : 2 points

Position 30 : 1 point

## 11. CANOPY RULES & CREW SAFETY

1. All X-CAT boats must be equipped with a Reinforced Canopy/Cockpit, it is defined as a containment area for the crew and should be constructed as an integral part of the boat or suitably attached to boat if a drop in cell is fitted this should be complete with reinforced shield/crunch zones. This Reinforced Cockpit Area must be designed and constructed to a specification capable of withstanding the forces of a water impact when running at the highest design speed of the boat.
2. X-CAT all new boats built after January 2010 must conform to the current UIM rules, All current boats to be assessed if water deflectors and crunch zones can be fitted. Any boat failing to carry out the Technical recommendations will not be eligible to race. All other canopy rules apply to the current boats.
3. New build manufacturers should contact the UIM for any planned rule updates due to the development program of canopy structure and safety designs.
4. As a recommended target for design and construction an impact pressure 20 tones per square meter should be met or exceeded, with the highest importance given to protecting the crew in the event of severe accident. Any specification or safety device over and above the requirements will be assessed and supported as required.
5. The extremity of a Reinforced Cockpit comprises of a canopy, forward and rear bulkheads, sides, floor, and water deflectors. Additionally the cockpit is to be protected by a reinforced shield fitted into the boat surrounding the cockpit with a cavity between to form a 'crunch zone'.

6. Mandatory Water impact deflectors surrounding the cockpit, designed with a shape and structure so that the water is diverted around the cockpit and away from the crew in the event of the boat being breached to be fitted. There shall be no openings within the exterior confines of the impact shield/crunch zone allowing the forced ingress of water.
7. A continuous fitted structural framework inside the cockpit must be installed reducing the unsupported panel area sizes of the cockpit cell lining. The framework will incorporate roll bars fore and aft of the hatch with extensions to support the screen aperture divisions, the rear bulkhead directly behind each seat and the top escape hatch flange. The flange is to be a minimum of 25mm width (New build boats must have a minimum of 50mm) measured across the flange recess, with the hatch opening measuring 0.55m by 0.825m at the widest points. The canopy apertures should be cut with all corners having a radius of minimum 25mm. The radius should be constant and have a smooth finish to relieve stress. The canopy aperture must have a 20 mm wide (minimum) fluorescent orange band around the opening. All boats are recommended to use the double flange method of installation as per the graphic below. All new build boats 2010 onwards must use the double flange installation.



8. There must be a minimum of 1 compression strut installed to support the canopy.
9. The main hatch being directly above the crew's helmets and its supporting flange must have further reinforcement to maintain its shape under stress and be able to withstand the impact of the water and retain its ability to function. The hatch thickness must be maintained throughout and not reduced at the flange where possible. The hatch should be protected by water deflectors incorporating internal and external method of prizing open the hatch caused by water pressure or binding to assist in emergency underwater rescue and escape.
10. The hatch shall be fitted with a catch which has a positive open and positive close mechanism and should hold the hatch against lateral forces. These catches shall be able to be opened from both inside and outside the cockpit and must have a second emergency mechanism to allow the rescue team to easily remove the hatch from outside if necessary. The hatch should be fitted with hinges with large bases and backing plates to spread the load and short release pins. This is important, because long pins invariably bind the hinge. It should be considered that the hinge centers be as far apart as possible. The release mechanism or hinges must not encroach within the canopy aperture area, and must not in any way hinder the exiting of crew members when fully race fitted. It is recommended that the catch have at least 2 locking points as far apart as possible, whilst still maintaining head clearance. It should be operated by a single handle with the addition of individual mechanisms internally in case of jamming. The hatch must be removable when the quick release hinge pins are removed. The locking mechanism must protect against both forward and aft stuffing
11. Mandatory secondary tunnel escape hatch, it is strongly recommended that polycarbonate windows are applied to the tunnel hatch to help illuminate the cockpit for ease of escape. Alternatively a waterproof light which is activated by water and/or being inverted with its own power source is fitted.
12. Canopy hatches and release handles, must be painted fluorescent orange or have a fluorescent orange background panel to identify them, with directional arrows to indicate the method of opening must be provided both inside and out.
13. There should be one or more divers' grab handles fitted to the outside of the top hatch.
14. The clearance between and around the crews helmet and the canopy must be a minimum of 120mm.
15. It is mandatory to close the canopy hatch, and for the hatch to remain closed during all racing and practice.



16. Raised sections above deck level, including hatches and openings, connected to the canopy, at least to the extent of the mandatory water deflection zones must be structural and comply
17. With the 'Reinforced cockpit area' rule. Any hatch within this area must have the same strength as the main cockpit hatch.
18. Cockpit seating must be side by side seating only.
19. The Cockpit should have flood tubes or other means of flooding the cockpit to equalize the pressure quickly in an accident. The floor of the cockpit should be as airtight as possible when in an upturned position. It is mandatory that sufficient buoyancy is provided in the boat, or in the material used for its construction, to ensure that the boat floats if capsized or holed, the hull should float as parallel with the surface of the water as is practical, to help in rescue accessibility. If extra buoyancy is needed, the buoyancy system described by the designer should be verified by the Measurer. This added buoyancy must be in at least four separate flotation units.
20. The various components that constitute the Reinforced Cockpit shall be properly maintained to ensure reliable operation of all components, with emphasis being placed on the canopy release mechanisms, emergency air supply and restraint systems, in accordance to the UIM cockpit guidelines.

#### Windscreens

1. Polycarbonate (Acrylic will not be accepted) areas are strongly recommended to be as small as possible, while still maintaining that the pilot and co-pilot have clear, safe and undisturbed visibility ahead at sea level whilst racing. It is strongly recommended that these polycarbonate areas are built using 12 mm thickness, or more.
2. The combined visibility the pilot and co-pilot must be through a horizontal arc of 225 degrees (112.5 degrees either side of the centre line of the boat).
3. These polycarbonate panels are to be recessed into the composite structure and should be bonded using a suitable bonding agent, and/or "bobbins" (Bonding the Windscreen is the recommended method of fitment. Boats built after January 2010 must use bonding only, to fit the windscreen no bobbins will be allowed)
4. It is highly recommended that there is also a through bolted outer flange for the fitting of the polycarbonate panels.
5. Screen flanges should be a minimum of 50 mm, if bobbins are to be used they should be fastened every 100 mm it is recommended to use metal bobbins" with heads, as opposed to the recessed plastic type.
6. The outer polycarbonate area of the flange fitting must not be painted, so that the measurer / scrutineer may monitor any discrepancies.

#### Seats and Seatbelts Side of head protection

1. Cockpits must be fitted with **rear and side** of head protection for each crew member. This must be an integral part of the seat, which must be attached directly to the structure of the Restraint Compartment. The head protection must be a minimum of 0.2m wide and extend at least 80% of the height of the safety helmet as worn by the crew whilst in the normal seating position. There must be a minimum of 0.12m vertical and lateral clearance between the canopy and each of the crewmembers when in the normal seating position.
2. The Restraint System must consist of a 5 or 6 strap harness and should utilise a 75 mm lap belt, a 50 mm strap over the shoulder harness rated at 4,100kg (9,000 lb.) and grommeted to prevent chafing or cutting of the belt. Harness straps must be attached directly to the cockpit structure. Those straps close behind the driver's head and neck must be 100 mm to 150 mm apart at point of attachment. The shoulder harness should be installed at 90 degrees to the spine at shoulder line to minimize compression injuries under high "G" loading. All straps must be free to run through intermediate loops or clamps/buckles. All anchor point bolts must be fitted with backing plates of 10cm minimum width. The driver harness attachment bolts in reinforced cockpits must consist of minimum grade EN8 bolts, with an 8 x 1.25 mm thread and locked nuts. There must be a spacer and plain washers on each bolt. The spacers must be glued to the cockpit structure. Intention of these spacers is to prevent buckling of surface material near bolts. This always leads to local delamination which easily spreads out over cockpit structure, when it is under stress.
3. On the sides of the structure, which has to take up the force on the attachment bolts, there must be a stainless steel plate (washer of minimum 3 mm thickness and 100 cm<sup>2</sup> area).
4. When using seats with suspension, and therefore not using a bulkhead restraint anchorage, drawings must be lodged with the National Authority of the measurer and approved prior to boat measurement.

5. All restraint systems must have a common method of release. The single lever method (sometimes called the NASCAR type) or rotary type, are both acceptable restraint release systems. Both types of restraint release must be examined for satisfactory operation by the scrutineer before every race.
6. The shoulder harness should be installed 90 degrees to the spine at shoulder line to minimise compression injuries and the high "G" loading. 75 mm minimum/maximum to Centre line of Lap Belt at Seat Back, Seat Bottom junction. Lap Belt should continue in straight line to anchorage.

## 12. DRAWINGS AND MEASUREMENT

Drawings of the Reinforced Crew Canopy/Cockpit to be supplied will include the following

1. The type of Canopy, Plan, side and elevation.
2. the Bulkheads and buoyancy system
3. Restraint System anchorage Points
4. Canopy aperture dimensions
5. The type and method and construction of release devices.
6. Material specification of the transparent areas.
7. Water impact deflectors and crunch zones
8. Material mechanical property data of the canopy, hatch, cockpit and crunch zones, running surfaces should be provided in support of the drawings to establish a database for future assessment and analysis.

Prior to Boat Measurement the drawing and material specifications shall be sent to the Measurer requested to measure the boat. On completion of measurement, the drawings and material specifications called for by the designer shall be lodged with the measurers National Authority before they issue a certificate of compliance and measurement. Drawings must also be submitted to the UIM for all entries to UIM races prior to racing.

The construction, strength safety and conformity to meet and exceed the recommended requirements is the sole responsibility of the boat owner and or boat builder. All points of impact incidence to the canopy, hatch and screens, (as being a primary impact area) cockpit and crunch zone extremities should be afforded the diligence deserved in protecting the crew. Any scrutineering or measurement by the UIM officials is not condition/construction survey.

Reference: Also 44.01 MEASUREMENT CERTIFICATE.

## 13. REINFORCED COCKPIT AREA AND CANOPY MANDATORY REQUIRMENTS

1. One single air supply (not oxygen) and a bottle will be provided for each riding crew member. The air supply must be securely fixed adjacent to, or on each one of them. It is recommended that sufficient air be provided in each individual bottle for ten minutes.
2. Air bottles must have a pressure gauge fitted for visual checking at pre-race scrutineering. This gauge should be filled with liquid and be at least 5 cm in diameter for easy reading.
3. Air supply bottles shall be "Turned On" before starting a race or taking part in practice and/or testing. All air hoses to be secured and mouthpieces to be placed within reach of the pilots and mounted on retainers
4. Diving Masks for each crew member, stowed securely and accessible.
5. A minimum of 1 belt cutter or knife is to be secured in the cockpit
6. Stop buttons/switches located in the cockpit area, immediately accessible to the crew and rescue officers. The stop buttons/switches must be identified by a fluorescent colour. These switches must shut off all fuel pumps as well as the ignition circuit.
7. A White High Intensity Strobe Light fitted to indicate "coming off the plane". The strobe light must be able to be operated by both crewman, and should be operated if a problem occurs, to enable any following race boats to take avoiding action. The strobe light shall be mounted on the top rear of the canopy. This strobe light may also be used as a substitute for the orange retirement flag when returning to port under reduced power.
8. A quick release steering wheel must be fitted on all boats but all pilots must be able to exit the cockpit without removing the steering wheel.
9. Rear view mirrors are mandatory, as well as a method of cleaning the canopy whilst underway.
10. Two fire extinguishers, each a minimum of 2kg must be carried and be readily accessible to the crew. All crew containment areas of inboard engine canopied boats must be fitted with a carbon-monoxide alarm.

11. Racing Vests - the efficiency of the racing vest is a matter of the exclusive responsibility of the wearer. Every crew member whilst on board must wear a racing vest during the practice runs and throughout the race. Racing vests must be colored high visibility orange or yellow. The racing vest must have epaulets/handles to help extract crew from the boat. The racing vest must have crutch straps or a method of ensuring that the vest does not "ride up".

#### Notes

"Cool Suits" - may contain any proven safe fluid or gas cooling agent other than Freon.

Eye protections - must be constructed of shatterproof material. Eyeglasses shall not be accepted as eye protection.

The Race Director - or Technical Director may prohibit use of any equipment he deems unfit for service.

Violation - of safety rules will result in a penalty assessed by the Race Director in conjunction with the Medical and Safety .

#### 14. OTHER REQUIREMENTS

Non-compliance - The Technical Director has the authority to deny entry to any race boat subject to these rules that has non-compliant cockpit safety systems. The Technical Director also has the authority to allow a non-compliant boat to compete, provided that after consultation with the Technical Director, the Technical Committee determines that the intent of the guidelines has been met and that the safety of the riding crew and fellow competitors is not jeopardised.

Intercoms - Locking boat intercom plugs connecting helmets to racing craft are expressly prohibited. Plugs must disconnect easily, placing no additional stress on competitors' necks. Inflexible metal or plastic intercom microphone booms are strongly discouraged.

Radios - A licensed ship-to-shore radiotelephone must be permanently installed in an unexposed location with a minimum power input of twenty-five watts with international marine VHF channels available and any channel designated by the Race Committee as being essential for race communications.

Bilge Pumps - Two (2) bilge pumps, at least one of which it is recommended shall be hand-operated. Electric bilge pumps are to be automatic and wired so as to maintain operation independent of the battery cut-off switches.

It is recommended that two Coast Guard Approved fully charged fire extinguishers with metal pull rings and an indication gauge showing amount of charge secured in a position are readily accessible to the crew.

Cleats - All cleats and other deck hardware must be recessed or protected.

Non-Skid - It is recommended that non-skid material be installed on the deck surface surrounding the cockpit to the satisfaction of the Chief Safety Inspector (Clear non-skid is available).

## APPENDIX 1

# RECOMMENDED SAFETY PROCEDURES AND GUIDELINES

### RECOMMENDED SAFETY PROCEDURES

01. GENERAL REQUIREMENTS
02. TECHNICAL RECOMMENDATIONS FOR SAFETY BOATS
03. INTERVENTION PROCEDURE

### SAFETY GUIDELINES

#### SAFETY

#### LAND SERVICES

- A. DRY PIT AND WET PIT AREAS
- B. FUELLING AREA
- C. HELICOPTER AREA (IF REQUIRED)
- D. HOSPITALITY AREA

#### ON WATER OFFICIAL TIME LAND SERVICES

#### SEA SERVICES

- E. RESCUE BOAT
- F. TOW BOAT
- G. FIRE BOAT
- H. COURSE PATROL BOATS
- I. FINISH BOAT
- J. COURSE CONTROL BOAT
- K. CONTROL BOAT

#### ACCESSORIES AND SPECIAL EQUIPMENT

#### SPECIAL INFORMATION TO BE PROVIDED BY THE LOC

## RECOMMENDED SAFETY PROCEDURES

Minimum requirements to run of an X-CAT Series Grand Prix

### 01. GENERAL REQUIREMENTS

- 01.01. the minimum depth of the water for the race course area is 4 meters in the turn mark; this depth can be reconsidered by the Safety Officer, in case of bad sea weather condition and or in case of particular geographical local condition;
- 01.02. the logistics point on land where safety boats and ambulances are located must not be further than 2 nautical miles from the furthest point of the race course;
- 01.03. the mooring post for the rescue boats has to allow for a parking area for ambulances;

### 02. TECHNICAL RECOMMENDATIONS FOR SAFETY BOATS

- 02.01. The rescue boats should be:
  - a minimum of 7.5 metres long with free bow in order to have enough space for stretchers;
  - sides and/or stern can allow an easy boarding of stretchers and/or must have a system for the easy loading of stretchers;
  - cannot transport on board persons who do not belong to the crew or to the rescue group;

- 02.02. Considering that race courses have a maximum lap length of six nautical miles, the safety plan, coordinated by the Local Safety Officer with the assistance of the Local Medical Coordinator, envisages a minimum number of six rescue boats. This plan is created according to the grid method in order to be able to patrol an area of 1 mile for each rescue boat.

Following this method, the rescue boats will be implemented if the race lap is longer than 6 miles.

The Local Safety Officer is responsible for the repositioning of the safety Plan, according to the interventions and in order to cover the whole race course.

The plan is not sufficient if 50% of the available rescue boats are used in one or more interventions at the same time.

In this eventuality, the Local Safety Officer must communicate the fact to the Race Director in order to stop the practice, pole position and/or race.

- 02.03. Each rescue boat is assigned a Tow boat and they form the safety group positioned in the grid plan. The rescue boat is the group leader.

The group has to take its position according to the safety plan and it has to maintain the position by maneuvering the engine (avoiding the creation of waves), at a minimum distance from the course line of 200 metres and, in any case, in a safe position.

Every group has to be constantly in contact and has to listen to Race Control on the assigned radio channel. The driver's assistant is assigned to carry out this function.



### 03. INTERVENTION PROCEDURE

- 03.01. Every safety group, composed of one rescue boat and one Tow boat, has to be in the assigned position at least 30 minutes before the official start time for practice, pole position & race.
- 03.02. Every group has to check how the practice is going, reporting every anomaly, accident, etc. to Race Control.
- 03.03. It cannot act unless authorised by Race Control.
- 03.04. Once the group has received the authorisation from Race Control, and if towing is not specifically requested - in which case, only the Tow boat would intervene - the group moves towards the point requiring intervention. While the rescue boat approaches the accident location, the Tow boat, with the yellow flag raised, places itself about 100 metres from the intervention point, on the course line, in a safe and visible position, signaling the danger to the other race boats, and protecting the intervention by the rescue boat.

If it is a medical intervention, the operation is controlled by the doctor and/or paramedic on board the Rescue boat, who will take care of rescuing the patient(s) and report to Race Control regarding what further action needs to be taken.

Once the rescue has been completed, the rescue boat will return to the established mooring post following the re-entry procedure, while the Tow boat will control the accident area and clear the water of any floating debris.

At the end of its intervention, with the consequent report having been made to Race Control, the group will take up its original position for normal service.

- 03.05. During the operation, Race Control will reposition the inactive groups in order to cover the whole race course.
- 03.06. If, during an intervention, the operation commander requires the aid of other rescue boats, Race Control will send the appropriate group(s), including the fire service.
- 03.07. If simultaneous interventions require the use of more than 50% of the active groups envisaged in the safety plan, it will be down to the Race Director, via Race Control, to stop the practice, pole position and/or race. In this case, the order will be given to raise the red flag, thus initiating the Race Stopped/Pace Boat procedure.

## SAFETY GUIDELINES

### SAFETY

The Safety and Rescue Plan includes both land and sea services.

All services will be carried out by Local Committee crews together with relevant class and Military crews.

The Safety and Rescue system will be co-ordinated by the Race Control.

### LAND SERVICES

The Land Safety System includes the medical, fire prevention, helicopter and hospitality area services.

The Land area is divided into four separate zones:

- Dry Pit and Wet Pit area;
- Fuelling area;
- Helicopter area;
- Hospitality area

The areas indicated above must be set up and equipped as follows:

#### A. DRY PIT AND WET PIT AREAS

##### A.01. Medical Services

The medical services will be carried out under the control of the Head of the Medical Committee, with:

- A.01.01. N.1 Standard Ambulance (“B” type), with doctor and paramedical crew, normal equipment.

This ambulance will be positioned close to Dry Pit, free to move, from 08.00 to 19.00 from arrival to departure of all vehicles.

##### A.02. Fire Prevention Services

- A.02.01. 3 Fire points each containing 3 Extinguishers of 30 KG in separate locations in the pit area.

- A.02.02. N.2 fire extinguisher operators.

- A.02.03. N.2 water pumps, positioned in crane area (one at each crane).

#### B. FUELLING AREA

##### B.01. Medical Services

- B.01.01 N.1 Reanimation Ambulance (“A” type), fully equipped and manned by reanimation doctor and reanimation paramedical crew.

This ambulance will be positioned close to Fuelling area, free to move, one hour before and until one hour after the official fuelling times.

- B.01.02 N.1 Fire engine with foam and a minimum of 2m3 extinguishing capacity, fully equipped and manned.

- B.01.03 N.2 fire extinguisher operators.

During fuelling operations, the area will be closed and controlled by the security crews.

#### C. HELICOPTER AREA (IF REQUIRED)

##### C.01. Medical Services

According to the local Law.

##### C.02. Fire Prevention Services

According to the local Law.



#### D. HOSPITALITY AREA

##### D.01. Fire Prevention Services

D.01.01. N.1 fire areas composed by 6 extinguishers of 10 Kg each, with locations clearly indicated.

#### ON WATER OFFICIAL TIME LAND SERVICES

N.2 Reanimation Ambulance ("A" type), fully equipped and manned by reanimation doctor and reanimation paramedical crew.

These ambulances will be positioned free to move one hour before and until one hour after the official race, practice and pole position times.

Note: The LOC can also use the fuel ambulance during the official race times.

#### SEA SERVICES

Safety and Rescue Services on Racecourse.

##### E. RESCUE BOAT

(Mandatory for Practice, Pole Position & Race)

E.01. A minimum of n. 6 Rescue Boats min 7 m, sufficient space in order to accommodate stretcher, with crew, n. 1 reanimation doctor or specialist paramedic and n. 2 divers for each boat, fully equipped for reanimation at sea (see following details).

##### F. TOW BOAT

(Mandatory for Practice, Pole Position & Race)

F.01. N.6 Boats with crew, equipped for towing in order to tow up to 5 tons (see following details).

##### G. FIRE BOAT

(Recommended for Practice, Pole Position & Race)

G.01. N.2 Boats with crew, equipped with anti fire system with foams and water pump.

##### H. COURSE PATROL BOATS

(Mandatory for Practice, Pole Position & Race)

H.01. N.6 / 7 Military boats for external course patrol.

##### I. FINISH BOAT

(Mandatory for the Race & Pole Position)

I.01. N. 1 Boat Fly bridge or other suitable craft for finish race procedure/pole position run (minimum 9m), with crew and Jury/Course Officials.

##### J. COURSE CONTROL BOAT

(Mandatory for the Race & Pole Position)

J.01. N.1 Boat each turn point with crew and n. 2 Course Officials (minimum 7m).

##### K. PRESS BOAT

(Mandatory 1 day before the first practice and until the GP day)

K.01. N. 1 Boat for X-CAT Press during whole period, available with driver at disposal of the Series Promoter (minimum 7m - high speed).

#### ACCESSORIES AND SPECIAL EQUIPMENT

- LOC DIVERS' EQUIPMENT

The only special request for the divers is the supply of 5 litre oxygen cylinders.

- LOC TOW BOAT

- X-CAT flag;
- Nautical radio system;
- 1 Yellow flag;
- 5 lengths of rope (15m each);





- 2 Blankets;
- 2 Marker buoys.
- LOC COURSE CONTROL BOAT
  - X-CAT flag;
  - Nautical radio system;
  - 1 Yellow flag;
  - 1 Red flags;
  - 1 Red flares.
- LOC FINISH BOAT
  - X-CAT flag;
  - Nautical radio system;
  - 1 Yellow flag;
  - 1 Red flags;
  - 1 Red flares;
  - 1 Chequered flag.
- LOC RESCUE BOAT

**CREW**

- Driver;
- Assistant;
- 1 Reanimation Doctor or Specialist Paramedic;
- 2 Paramedic Scuba Divers

**LOC RESCUE BOAT EQUIPMENT**

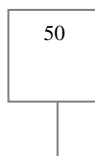
- X-CAT flag;
- Nautical radio system;
- 1 Yellow flag;
- Oxygen
- 2 Collars
- C35
- Several Splints
- Pair of Scissors
- Floating Stretcher
- Thermal Blanket
- Emergency Medical Bag (see following details)

- SUGGESTED BAG'S CONTENTS IN DETAIL (FOLLOWING THE NATIONAL LAWS)

Venflon 14 g	Ipnovel 15mg/3ml	Effortil vials 10mg/1ml
Venflon 16 g	Cortop vials 500mg/5ml vials	Ebrantil vials 50mg/10ml vials
Venflon 18 g	Gluc 33%/10ml	Sodium bicarbonate 8.4%/100ml
Butterfly 19 g	Mgso4 vials 1g/10ml vials	Emagel 500ml/flac
Butterfly 21 g	Normal saline solution 0.9%/10ml bottle	Sodium chloride 0.9%/500ml
Arterial compressors with 19g Needles	Calcium chloride fl 1g/10ml	Corrugated tube
Silk bandage 2.5	Esmeron vials	Endotracheal tube 7.5
Syringes 5 millilitre	Silk bandage 2.5	Endotracheal tube 7
Syringes 10 millilitre	Lubricant	Endotracheal tube 6.5
Administration set with flow regulator	Laryngoscope	Endotracheal tube 6
3-way taps	Blades for laryngoscope Mac 3-4	Endotracheal tube 5.5
Mini tracheal set	Magil forceps	Disposable gloves, size M
Adult laryngeal tubes	Mayo cannulas in several measures	Adrenaline 1mg/ vials
Adult Ambu bag	Intubation stylet	Midarine (succinylcholine) vials
Adult hematometer	Atropine sulphate 0,5mg/1ml vials	
Adult ventilation mask	Xilocaina 2% 50ml flac	
Ventolin spray flc	Cordarone 150mg/10ml	
Diprivan 10mg/millilitre bottle	Revivan vials 200mg/	

SPECIAL INFORMATION TO BE PROVIDED BY THE LOC

- 1) NAME OF PREWARNED HOSPITAL(s) WITH CONTACT NAMES OF DOCTORS AND TELEPHONE NUMBERS;
- 2) NOTIFICATION OF LOCATION OF SPECIALISED UNITS AT HOSPITAL(s) (Emergency surgery; Radiology with TAC; Resuscitation; etc.);
- 3) TIME TO COVER DISTANCES FROM THE PITS TO THE DESIGNATED HOSPITAL(s) VIA ROAD;
- 4) TIME TO COVER DISTANCES FROM THE PITS TO THE DESIGNATED HOSPITAL(s) VIA HELICOPTER;
- 5) FURTHER EMERGENCY BERTHING POINTS (IF ANY);
- 6) POSSIBILITY OF SECURITY PRESENCE AT SUCH BERTHING POINTS;
- 7) POSSIBILITY OF RESUSCITATION AMBULANCE AT SUCH BERTHING POINTS.



## APPENDIX 2

# ON-WATER PROCEDURES AND GUIDELINES

01. POLE POSITION PROCEDURE
  - 01.01. GROUPING
  - 01.02. POLE POSITION PROCEDURE
02. PIT STOP PROCEDURES
03. START PROCEDURE
04. RACE STOPPED/PACE BOAT PROCEDURE
  - 04.01. RACE STOPPED
  - 04.02. PACE BOAT PROCEDURE
05. FINISHING PROCEDURE

### 01. POLE POSITION PROCEDURE

#### 01.01. GROUPING

All Race boats must be positioned on the wet pit pontoon 1 hour before the scheduled start time for Pole Position, unless otherwise stated in Race instructions or race bulletin. Boats to be positioned in Championship order.

#### 01.02. POLE POSITION PROCEDURE

X-Cat Pole Position session is mandatory with no minimum or maximum number of laps. Boats qualify in order of fastest recorded lap times during this session. Any boat failing to compete in Pole Position session will be placed after all the qualifying boats from the session in their championship order.

As soon as the green flag is displayed and the course is open, race control will call in championship order, each boat to give permission to enter the course. After this first call any race boat is entitled to request permission from Race Control on the designated VHF channel to leave the Race wet pit pontoon and enter the race course (a white flag may also be shown 10 minutes prior to start of pole position).

There is no maximum number of race boats on the course.

Any boat entering or leaving the race course must request explicit permission from Race Control via radio on the designated VHF channel and follow the In & Out procedure as given in the Race Instructions and/or Bulletin and/or Pilots' Briefing.

The course will be closed at the Race Instruction specified time min 45 minutes to 90 minutes maximum, the red flag will be raised and all boats on the course must exit the course following the In & Out procedure.

As a consequence of the strict time limitation, all race boats must commence a timed lap before the allotted minutes have passed so as to be entitled to finish this timed lap. Each boat must then proceed to the wet pit area, respecting the In & Out procedure.

The final Pole Position results will determine the starting order for all participating boats in the Grand Prix Race.

It is at the discretion of the Race Committee to establish whether, when entering the race course, the departure point will be the Wet Pit, or whether it is necessary to designate a Pole Position Milling Area. This will be indicated in the Race Instructions and/or Bulletins and/or Pilots' Briefing.

After the start time of the Pole Position, priority use of the cranes is at the sole discretion of the Technical Director.

No refuelling will be allowed during the Pole Position.



Under no circumstance during the Pole Position may any boat run outside of the Pole Position course area. Failure to comply will result in a fine of Euro 1,000 and/or disqualification.

Lap timing commences after the first passing of the timing gate.

After Pole Position all boats must be available for scrutineering at discretion of the Technical Director.

The result of the Pole Position will be determined by the fastest timed lap.

No part of the Pole Position is subject to protest.

In addition to the penalties determined by the Rules, in the case of missing or destroying a buoy, the time from the relevant lap will not be counted towards qualification; but does count as one of the allowed number of race laps.

In the event of bad weather or curtailment

In the event that the Pole Position has run for a minimum of 50% of the published duration the results will stand, any boats that have not chosen to run in that time will be classified after the qualifying boats in their Series Championship order.

The session is cancelled if it is not to completion of 50% of the published time, in this case the Race start order will follow the Championship order. In case of new Teams Entry, their race start position will be taken in base at the Championship / Race date entry list.

## 02. PIT STOP PROCEDURES

- Boats which wish to make a Pit Stop must follow the In&Out procedure outlined in the Race Instructions, Drivers' Briefing and/or Bulletin
- Boats returning to the Wet Pit pontoon must turn on their strobe light.
- If several boats are returning at the same time, use of the cranes will be designated by the Technical Director based on the order of the boats entering the Wet Pit pontoon.
- Boats must be at very slow speed at all times while in the harbour (no wake). Penalty for infringement: Disqualification and/or fine.
- Refuelling is not permitted.
- Only the official crane areas may be used during pit stops unless agreed by the Technical Director.
- No boats will be recovered during the duration of Pole Position, except for damaged boats and under the responsibility of the Technical Director.
- Cranes may be used to recover boats during the Grand Prix race period, but priority will be given to pit stops, except for damaged boats, and under the responsibility of the Technical Director.
- Craning time is fixed at ten minutes per Pit Stop. A boat can enter the craning area only when it is ready to be hoisted by the crane. Craning time starts from when the boat is attached to the crane by its straps and ends when the straps are released from the crane.

See also Event Rules Section H 31 Craning and 32 Pit Stop Rules.

## 03. START PROCEDURE

All boats will be positioned in Pole Position order by the wet pit pontoon.

15 minutes before the start, a white flag will be raised in the wet pits and the drivers will take their positions in their cockpits with their attention focused on race boat VHF channel.

10 minutes before the start, the green flag will be raised and the Race Director / or Start Marshall will call by radio the race boats in Pole Position order; all boats will then leave the pontoon accordingly, following the Start Boat to proceed on parade to the start.

All boats will follow the Start Boat at very low speed (max 10 knots) in Pole Position qualifying order. Where possible a parade lap will take place straight in to a rolling start, with no Flare.

When the Start Boat turns in the direction of the Start Line, then all race boats have to take their position at the start.

All boats will line up in Pole Position qualifying order as described. The Pole Position winner must be adjacent to and minimum 30 metres astern of the Pace Boat. All other boats must line up alongside the Pole Position winner in PP order and maintain a distance approx 30 metres between them. When line up it's complete, a yellow flag will be raised. When all boats are in position the green flag will be raised as soon as possible in the vicinity of the start chute.

A second start boat may be used where available to form an imaginary line between the 2 start boat transoms. In this instance the 30 metre rule will apply from both start boats.



If any of the race boats are out of Pole Position order by the Start, a long lap penalty will be applied.

The Start Chute may be marked by two buoys. All boats must pass through this Gate.

The start of the race is denoted when the Start Boat has raised the green flag. The green flag will be raised at approx 45 Mph and confirmed by radio. The start boat will not advise boats to speed up or slow down, this is the sole responsibility of the pilots.

All race boats must line up and remain at a constant speed in starting order – no ‘runs’ through the line up will be permitted. Penalty – Long Lap.

If a race boat starts before the yellow flag is raised, the penalty is Disqualification.

Once the green flag is raised and the race is started all boats are under ALL RACE RULES and all overtaking and rounding marks rules will apply.

It is the Team Manager's responsibility to guarantee the good functioning of the Race Boat's radio communications.

Penalties for infraction of the Starting Procedure are:

- Interference with starting procedure – Long Lap Penalty and if the driver had caused danger, a Yellow Card.
- Should the bow of any boat be in front of the transom of the start boat when the green flag is raised - Long Lap Penalty or time penalty 30 seconds.
- Starting on the incorrect side of the start boat or in front of the start boat - Disqualification.
- Damaging the position of another boat - Yellow card.
- See relevant diagram issued with Race Instructions.
- Infringement of the 30 meters rule will be penalized with one long lap penalty or time penalty as describe in the Race Instructions.
- See relevant diagram issued with Race Instructions.

NB - Refer to the race course location, bad weather condition, or others, different start procedure can be applied by Race Director.

The Start Can be as follow:

1. Standard one line;
2. Standard one line with multi gates on the first turn mark;
3. Multi line;
4. Single Row

See Appendix 5 – Start Procedures Drawing & Clarification

#### 04. RACE STOPPED/PACE BOAT PROCEDURE

All Races – In the event of a race boat capsizing racing will be immediately Red Flagged until the incident is under control. Pace boat procedure will then commence as per rule and/or race instruction.

##### 04.01. RACE STOPPED

In the event that a race has been stopped with a red flag, the race time will keep on running.

If the race is stopped during the first lap or, in any event, before the first timed lap has been completed by the leading boat, the Restart will be effected in accordance with the normal Start Procedure and starting order as given in the Race Instructions and/or Bulletin and/or Pilots' Briefing.

If the race is stopped before the first timed lap has been completed by the last boat, all boats must proceed to the Pace Boat Milling Area. If the Pace boat raise Nautical Flag Code “Y” means follow the Pace Boat and further instructions will be communicated via radio race channel or visual signals.



In any case, whichever procedure is to be followed (normal Restart and/or Pace Boat), all boats must observe the Pace Boat Milling Area procedure as described below.

- The stopping of the race will immediately be communicated via radio (VHF channel as announced in the Race Instructions) and via timing computer.
- It is the responsibility of the Team Manager to inform his pilots that the race has been stopped.
- All boats must immediately slow down. No overtaking will be permitted from the stopping of the race/red flag.
- Slowly, each boat must follow the race course to the designated Pace Boat Milling Area where the Pace Boat is located and waiting.
- As soon as all race boats are assembled in the Pace Boat Milling Area they must follow the instructions from the Pace Boat:
  - If the Pace Boat shows the red flag, the race is definitively stopped and all boats must follow the Pace Boat into the wet pits.
  - If the Pace Boat raised the yellow flag and/or turns on the orange strobe lights, the Pace Boat procedure comes into force.

Refer also to Event Rules 35 Finishing Procedure 01, 02, 03.

#### 04.02. PACE BOAT PROCEDURE

The pace boat and the location of the Pace Boat Milling Area will be announced in the Race Instructions and/or Drivers' Briefing and/or Bulletin.

- Once all race boats have reached the Pace Boat Milling Area and are assembled, and the Pace Boat raised the yellow flag and/or turns on the orange strobe lights, the race boats must line up behind the pace boat in the order of the classification of the lap prior to the stopping of the race. From this moment on, each single overtaking will be penalised with 1 long lap per boat overtaken. (3 boats overtaken under Red Flag = 3 long laps as penalty).
- Race Control will announce the line-up via radio and timing screen.
- As soon as all race boats have taken their correct position behind the pace boat, they must closely follow the course of the pace boat.
- From the moment that the race course is cleared, the pace boat will follow the regular race course (race lap) as announced in the Race Instructions and/or Pilots' Briefing and/or Bulletin.
- It is the responsibility of the pace boat/Race Control to restart the race. The pace boat will accelerate to a high speed. The race boats must continue to follow the pace boat in the designated order.
- The restart of the race will be indicated by a green flag and communication via radio.
- The race will be considered restarted either as soon as the pace boat, at any time, when the Green flag is raised and called on the Raceboat channel.  
The green flag will be displayed and the Team Managers will be informed via radio and timing screen  
Overtaking is only permitted after the restart.

In the case of a boat having technical or other problems during the restart procedure, the strobe light must be switched on immediately and all following boats are entitled to pass this boat without penalisation. If the strobe lights are not switched on immediately and the restart procedure is affected, the concerning boat will be penalized with 1 long lap / or additional Time.

If the boat is not able to perform this penalty during the actual race, the penalty will be valid for the next race in which the boat takes part.

#### 05. FINISHING PROCEDURE

- 05.01. No Patrol Boat or Jury Boat shall be released from his station until the official in charge at Race Control has given clearance to that specific boat.
- 05.02. Once all the race boats have passed the Finish/Timing line, they must proceed to a safe area as defined in the Race Instructions.
- 05.03. Once all the boats have passed the Finish/Timing line, the race boats are free to return to the wet pits, following the In & Out Procedure in the Race Instructions and/or Bulletins and/or Pilots' Briefing.
- 05.04. The Winner, 2nd- and 3rd-placed boats will be led to the Wet Pits or other designated area as announced in the Race Instructions by the Start/Pace Boat.



## APPENDIX 3

# ON-LAND PROCEDURES AND GUIDELINES

- 01. PITS
  - 01.01. DRY PIT
  - 01.02. CRANING AREA
  - 01.03. REFUELLING AREA
  - 01.04. WET PIT AREA
  - 01.05. HELIPAD
  - 01.06. TIMING/TEAM MANAGERS' AREA
- 02. HOSPITALITY AND VIP AREA
  - 02.01. PODIUM AREA
- 03. OFFICES
  - 03.01. PROMOTER OFFICES
  - 03.02. X-CAT OFFICIALS
  - 03.03. PRESS OFFICES
    - 03.03.01. PROMOTER X-Cat Press - PR
    - 03.03.02. LOC Press Office
  - 03.04. X-CAT TV / VIRTUAL SYSTEM
  - 03.05. MEETING ROOM
  - 03.06. PILOTS' BRIEFING ROOM

### 01. PITS

The PIT area comprises the areas on land and the quayside where the activities connected to the event take place.

The Pit area is made up of: Dry Pit; Craning Area; Refuelling Area; Wet Pit; Helipad; Timing area;

#### 01.01. DRY PIT

The Dry Pit is the area where the race boats and workshops are situated.

The usable area required is c.3,000m<sup>2</sup> and it is requested that this be available from 3 days before the start of the race weekend until the day after the Grand Prix.

It is compulsory that this area be closed off and the entrances protected 24 hours a day by security guards (the type of pass which allows access will be advised by Race Promoter via panels displayed in the area).

The area must have sufficient space for the movement of trucks to and from the craning area (point 1.2) and the refuelling area (point 1.3)

The Dry Pit must be equipped with the following:

- a) 8 multiple (minimum dual) 220V electricity connections with a maximum charge of 50 kW;
- b) 8 multiple (minimum dual) water supply points;
- c) minimum of 4 x 200 litre containers for collecting oil;
- d) 1 rubbish bin for each race boat (minimum of 10), 200 litres in size;
- e) 1 fire extinguisher for each race boat (minimum of 10) and a minimum of 2 fire prevention officers;
- f) 1 forklift truck, minimum 3 tons (no operator);
- g) 1 van for transporting materials (no driver);



- h) adequate electric lighting;
- i) adequate toilet facilities.

#### 01.02. CRANING AREA

The craning area is where the cranes for launching and haulage operations are situated.

The usable area required is c.1,000m<sup>2</sup> and it is requested that this be available from 00.00 on the day 2 days before the start of the race weekend until 24.00 on the day of the Grand Prix.

It is compulsory that the entrances to this closed off area be protected 24 hours a day by security guards (the type of pass which allows access will be advised by Event Promoter/Organiser via panels displayed in the area).

The area must have sufficient space for the movement of trucks to and from the Dry Pit (point 1.1) and the refuelling area (point 1.3).

The Craning Area must be equipped as follows:

- a) 2 x minimum 30 ton cranes with 20-metre range available for use and in position from 08.00 of the day before the race weekend until 24.00 of the day of the GP, with operators present during the official hours. The operators must also be on call for other activities, if necessary, at other times.
- b) 2 multiple (minimum dual) 220/380V electricity connections
- c) 2 water pumps, one by each crane, to be used to pump out race boats in case of accident
- d) a minimum of one 200 litre rubbish bin;
- e) 2 fire extinguishers for each crane;

#### 01.03. REFUELLING AREA

The refuelling area is where the race boats fill up with petrol.

The usable area required is a minimum of c.200 m<sup>2</sup> and it is requested that this be available from 00.00 on the first day of the race weekend until 24.00 on the second day of the race weekend.

It is compulsory that this area be closed off and the entrances protected 24 hours a day by security guards (the type of pass which allows access will be advised by Promoter/Organiser via panels displayed in the area).

The area must have sufficient space for the movement of trucks to and from the Dry Pit (point 1.1) and the craning area (point 1.2).

The Refuelling Area must be equipped as follows:

- a) 1 fire engine equipped for combustibles with personnel on call during all official refuelling times (the service must start 45 minutes before the official period and last until 45 minutes afterwards); The fire prevention group must also be on call outside these hours;
- b) 1 resuscitation ambulance with medical staff and paramedic in attendance during all official refuelling times (the service must start 45 minutes before the official period and last until 45 minutes afterwards);
- c) 1 multiple (minimum dual) 220/380V electricity connection;
- d) a minimum of one 200 litre container for collecting oil/fuel;
- e) 4 x 30 Kg fire extinguishers;
- f) sufficient antipollution material.

#### 01.04. WET PIT AREA

The Wet Pit is the area for the launching and haulage of the boats.

The usable area must be adjacent to the craning area and have sufficient space for the positioning of c.20 X-CAT racing boats measuring 10 metres each.

It is compulsory that the entrances to this closed off area be protected 24 hours a day by security guards (the type of official pass which allows access will be advised by Promoter/Organiser via panels displayed in the area).

The area must be equipped with gangplanks, ladders, etc. which enable the authorised personnel to reach the pontoons.





#### 01.05. HELIPAD

The helipad is the area from where the helicopters used for the race take off, land and park.

The entrance to this closed off area must be protected by security.

The usable area must be suitable for the use by 3 helicopters (e.g. EC 130 or similar).

As an alternative to a permanent helipad, it is possible for the Local Organiser to provide an area which can be used for this purpose on a temporary basis and request the "OPENING OF TEMPORARY HELIPAD" authorisation from the relevant Civil Aviation Authority.

In this case, the Local Organiser must arrange for the temporary area to be set up in the following way or, in any event, in accordance with all legal requirements.

#### 01.06. TIMING AREA

The Timing Area is where the timing of the event is done and from where the participating Teams are controlled.

It is situated close to the Finish Line.

The usable area required is a minimum of c.20 m x 4 m and it is requested that this area be available from 00.00 on the day 3 days before the start of the race weekend until 24.00 on the day of the Grand Prix.

It is compulsory that this area be closed off and the entrances protected 24 hours a day by security guards (the type of pass which allows access will be advised by Promoter/Organiser via panels displayed in the area).

The Timing Area must be equipped as follows:

- a) Use of Power Electricity: 10 multiple (minimum dual) 220V electricity connections; Min 10 KW;
- b) 4 MB upload Internet Connection;
- c) 3 rubbish bins;
- d) 2 fire extinguishers;

#### 02. HOSPITALITY AND VIP AREA

The Local Organiser should reserve an area to be used as Hospitality and VIP Area (for approximately 100-150 persons (Teams – Organiser- LOC- Sponsors, plus Guests).

It is requested that this be available from the day before the race weekend until the end of the race weekend (opening from 09.00 am till 19.00 h).

This area has to be organized with catering service for Teams-Organiser- LOC- Sponsors and Guests.

#### 02.01. PODIUM AREA

The Podium Area is where the prize-giving takes place.

This is normally situated inside the VIP Hospitality area and where VIP activities are run.

The usable area required is c.100 m<sup>2</sup> and it is requested that this be available from 00.00 on the day before the start of the race weekend until the end of the prize-giving ceremony which follows the Grand Prix.

This area must be protected by security guards for the duration of the prize-giving ceremony (the type of pass which allows access will be advised by the Promoter via panels displayed in the area).

The Podium area must be equipped as follows:

- a) 1 multiple (minimum dual) 220V electricity connection
- b) 1 stage, minimum 8m wide, 6m deep, on which backdrop is to be set up (provided by Promoter/Organiser) steps up to stage
- c) 1 PA system with radio-microphones (2)
- d) Table to display trophies



### 03. OFFICES

The Offices are a collection of areas requested for the execution of the technical and administrative work relating to the event.

They comprise: Promoter Operations Office; X-CAT Officials; X-CAT Press/PR; X-CAT TV; X-CAT Virtual System; Press Office; Meeting Room; Briefing Room;

Overall, they can be made up of offices and/or portacabins/stretched flexible structures set up for the purpose.

The Offices are normally located near the Pit Area.

They should be cleaned, with waste paper baskets to be emptied, on a daily basis.

#### 03.01. PROMOTER OFFICES

Made up of 1/2 rooms totalling c.30m2:

- a minimum of 5 workstations, made up of desks and chairs with sufficient electrical sockets and internet connections (either via cable or wireless); one fax machine; one phone line; one colour photocopier with sorter; one colour and several b/w printers; also area to be used for private meetings.

#### 03.02. X-CAT OFFICIALS

Made up of 1 room totaling c.20m2.

An office with a minimum of 8 workstations, made up of desks and chairs with sufficient electrical sockets and internet connections (either via cable or wireless); one fax machine; one phone line; one colour photocopier with sorter; one colour printer;

#### 03.03. PRESS OFFICES

The Press Offices are divided between International (Promoter) and Local (LOC).

The Promoter Press/PR- and the local Press Office must be one large room, separated by a room divider.

##### 03.03.01. Promoter X-CAT Press - PR

An office c.25 m2, to seat a minimum of 3 people, with workstations made up of desks and chairs with sufficient electrical sockets and fast ADSL Broadband internet connections (either via cable or wireless); one fax machine; one telephone line;

##### 03.03.02. LOC Press Office

An office c.60 m2 (size, tables, chairs, etc. depend upon number of journalists expected) equipped with 5 telephones, one fax, ADSL Broadband internet connections, colour photocopier, a minimum of 4 computers, printers;

#### 03.04. X-CAT TV / VIRTUAL

A closed off office, minimum 40m2, equipped with a minimum of 4 workstations, sufficient electrical sockets, internet connections (either via cable or wireless AT LEAST MINIMUM 4 Mbit UPLOAD); one fax machine; one telephone line; computers with CD/DVD writer;



### 03.05. MEETING ROOM

A room which can seat up to 30 people for private X-Cat /Promoter meetings, furnished with tables and chairs.

### 03.06. PILOTS' BRIEFING ROOM

A well-presented room which can seat around 50 people for the Pilots' Briefing. It is set up with a top table (for a minimum of 8 people) and seating in theatre-style.

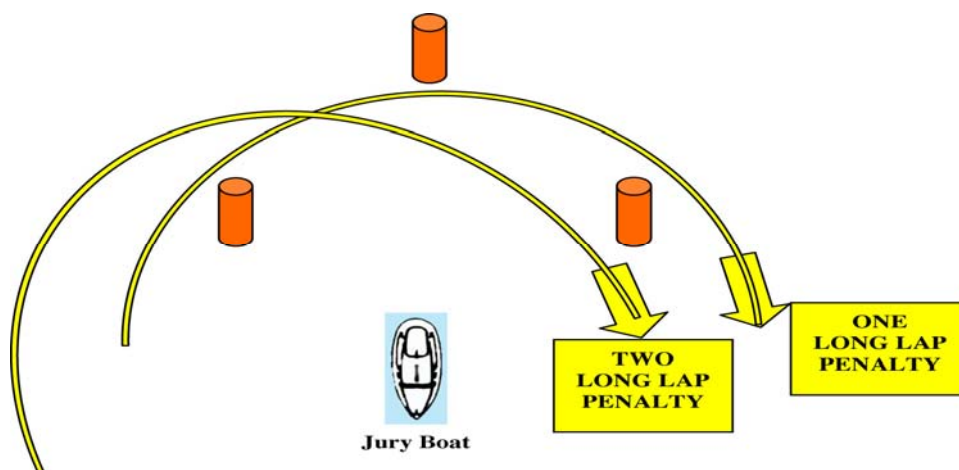
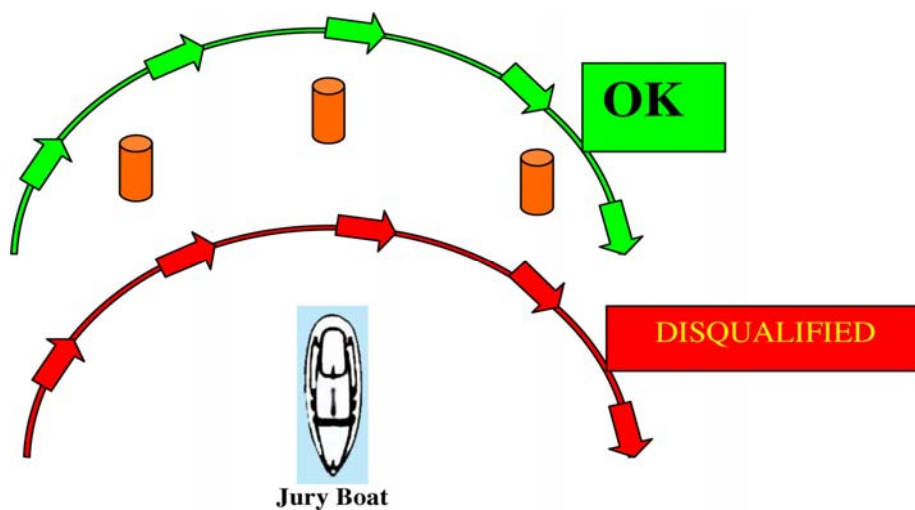
For use for two hours on the first day of the race weekend.

Equipped with PA system with radio-microphones (2), overhead projector and PC projector with projection screen.



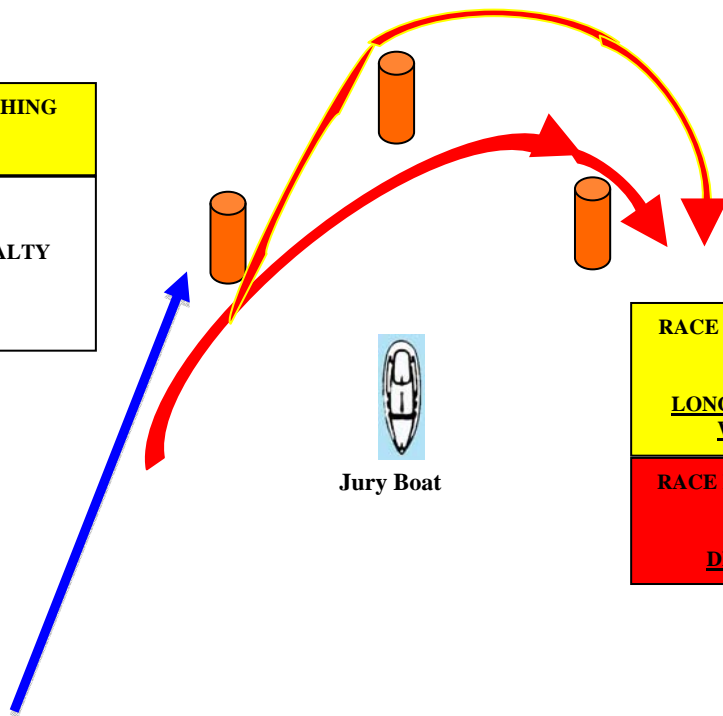
APPENDIX 4

TURN BUOYS CLARIFICATION DRAWINGS



**RACE BOAT PUSHING**

ONE  
RACE LAP PENALTY  
PLUS



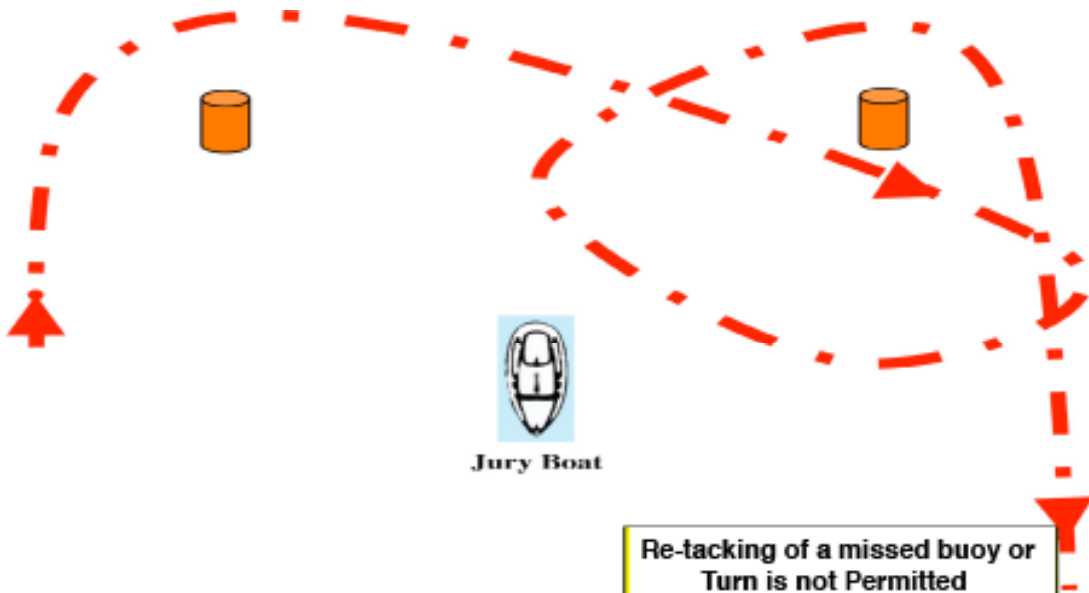
Jury Boat

**RACE BOAT MISS 1 OR MORE  
TURN BUOYS**

LONG LAP PENALTY RULE  
WILL BE APPLIED

**RACE BOAT MISS FULL TURN  
BUOYS**

DISQUALIFICATION

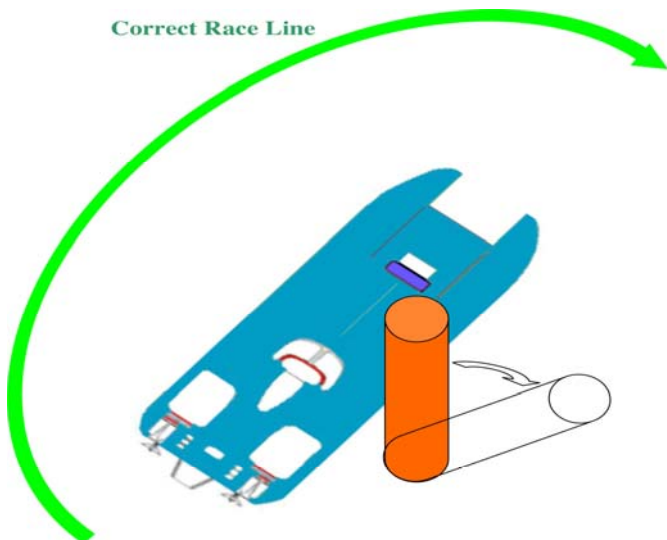


Jury Boat

**Re-tacking of a missed buoy or  
Turn is not Permitted**

**DISQUALIFIED**

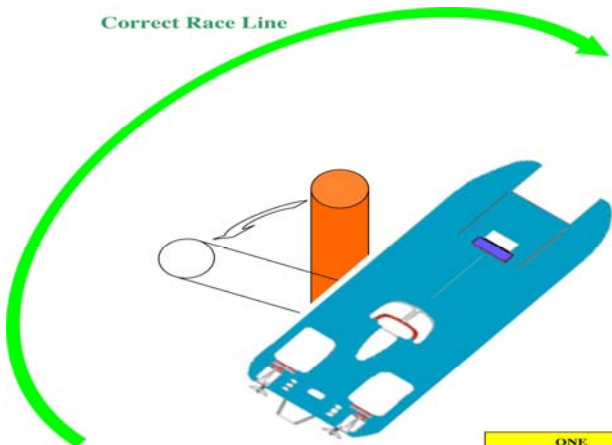
Correct Race Line



**FINE**  
**1.500,00 EURO**  
if buoy destroyed

**OK**

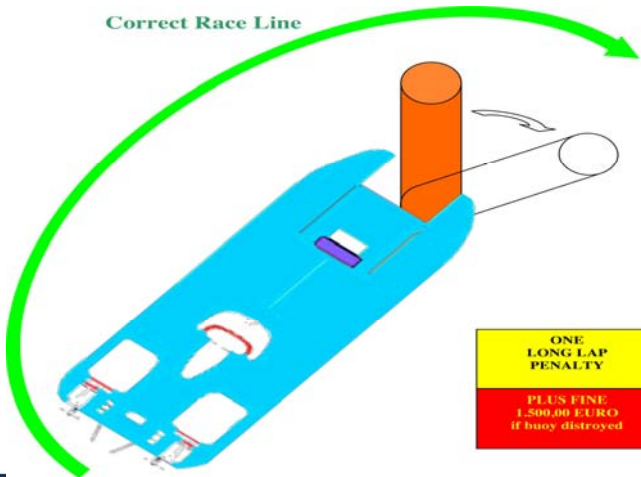
Correct Race Line



**FINE**  
**1.500,00 EURO**  
if buoy destroyed

**ONE  
LONG LAP  
PENALTY**

Correct Race Line

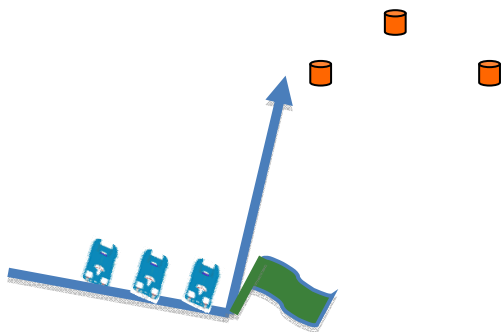


**ONE  
LONG LAP  
PENALTY**  
**PLUS FINE  
1.500,00 EURO**  
if buoy destroyed



# APPENDIX 5

## START PROCEDURE DRAWING & CLARIFICATIONS



Standard Start Procedure

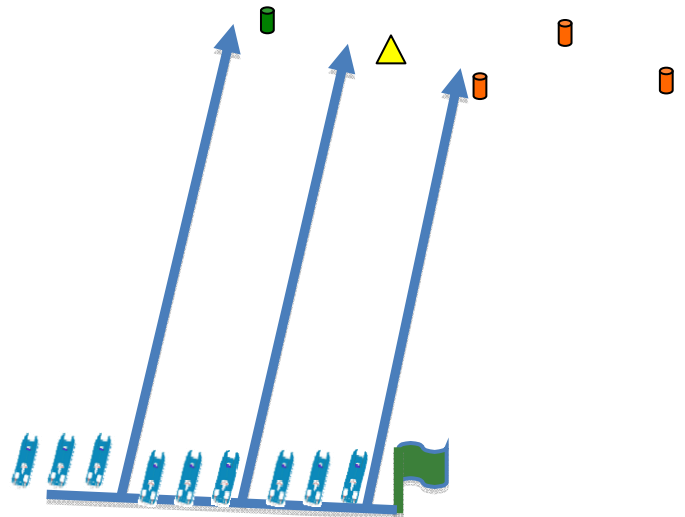
- ALL
- Race
- Overtaking

Standard Start Procedure  
On line with multi gates on the first turn mark

ALL RACE BOAT MUST MAINTAIN THEIR REFERENCE BUOY TURN MARK

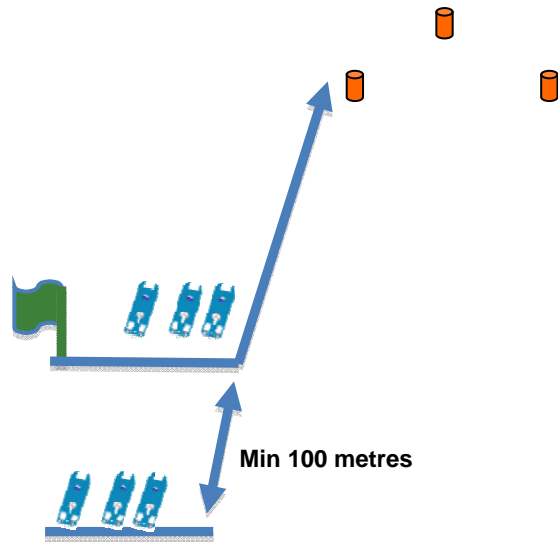
- ALL
- Race
- Overtaking
- Rounding marks

Rules will apply inside each "gate channel"



**Start Multi Line Procedure**

ALL  
Race  
Overtaking



**Single Row Start Procedure**

(Valid also for Pace Boat re start procedure)

**ALL RACE BOAT MUST MAINTAIN THEIR REFERENCE BUOY TURN MARK**

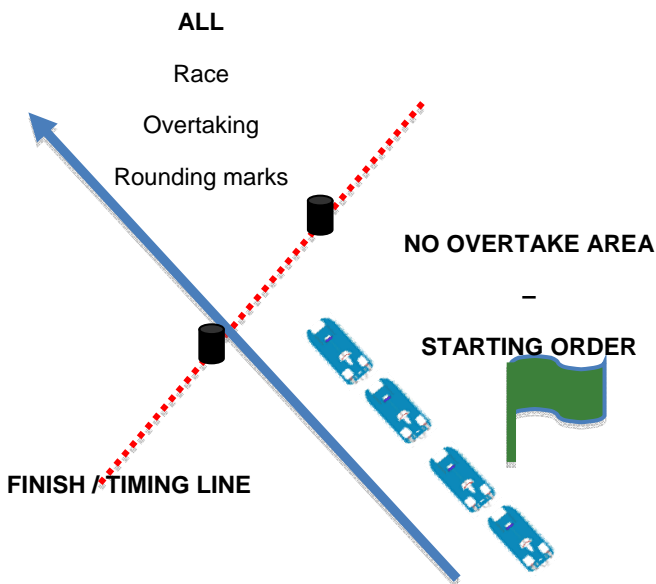
**Procedure**

In the case of single row start, the race boats must be in line respect their starting order.

Boats must maintain their respective position behind the Pace Boat

It is the responsibility of the Pace Boat to raise the green flag as soon as the race course is cleared and all conditions are ready for re start.

**Note: (This Schematic is only for clarification. Reference Appendix 2-04.02)**





# APPENDIX 6

## ADVERTISING & BRANDING CODE

### 1. Definitions

1.1 The following definitions shall apply to this UIM X-CAT Advertising Code only:

"Advertising" a name, logo, slogan, description, depiction, a variation or distortion thereof, or any other form of communication that promotes an organization, person, product, service, brand or idea so as to call attention to it or to persuade persons or organizations to buy, approve or otherwise support it

"Boat Number" an identifier assigned to a boat by the Series Promoter which shall be displayed on the bow of that boat.

"Competitor" a person on board a boat while Racing.

"Racing" the period of time defined in the Race Instructions.

1.2 Advertising on a boat, personal equipment, race overall, or any other object on board a boat shall be displayed in accordance with this Code, the applicable UIM X-CAT rules or the rules of the relevant system. Advertising that is not specifically permitted under this Code is prohibited.

1.3 This Code applies to racing boats – Teams and Competitors during: Official Practice – Pole Position and Racing and at all other times when so prescribed herein.

1.4 Any Advertising and anything advertised shall meet generally accepted moral and ethical standards. Any Advertising which is political, religious, racial or propaganda shall not be displayed on a boat, personal equipment or any other object on board a boat while Racing. Attention is also drawn to the laws of individual nations which may restrict Advertising within their territory or territorial waters.

1.5 Advertising on the boat shall be clearly separated from hull/boat numbers and from X-Cat – UIM/WPPA insignia and the Event Organizing Authority Advertising

### 2. Team/Competitor's Advertising

2.1 Each Team or Competitor, with the agreement of the Person in Charge, may display Advertising on personal equipment.

2.2 Subject to any limitations on Advertising in the applicable UIM X-CAT Rules or the rules of the relevant System, Advertising chosen by the Person in Charge may be displayed on a boat /race overall or part thereof, except on the areas detailed in Regulation 1.1 – 1.2

### 3. Event Advertising

3.1 The Organizing Authority may require that boats to display the Event Advertising, provided that such requirement is stated in the Advance Notice or Race instructions. The Organizing Authority must provides the boats with the needed materials (stickers).

### 4. UIM Logo and Branding Guidelines:

Teams / Competitors and event organizers are not allowed to use the UIM logo for any commercial activities or any use which is not specified by the Series Promoter. The usage of the UIM logo when authorize within this code should follow the UIM Branding Guidelines available on the UIM website: [www.uimpowerboating.com](http://www.uimpowerboating.com)



**5. WPPA & X-CAT Logo And Branding Guidelines:**

Teams / Competitors and event organizers are not allowed to use the WPPA and X-CAT logo for any commercial activities or any use which is not specified by the Series Promoter. The usage of the WPPA logo when authorize within this code should follow the WPPA and X-CAT Branding Guidelines available on the X-CAT website: [www.xcatracing.com](http://www.xcatracing.com)

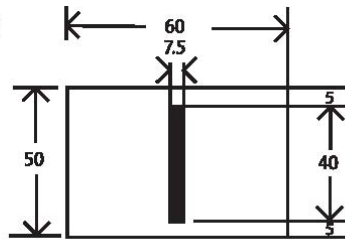
**6. Race Number Dimensions:**

The Race number has to be contained in a white square of 60 x 50cm for numbers 1 to 9, a white square of 70 x 50cm for numbers 10 to 19 and two digit numbers containing a 1, and a white square of 80 x 50cm for numbers 20 to 90 except two digit numbers containing a 1.

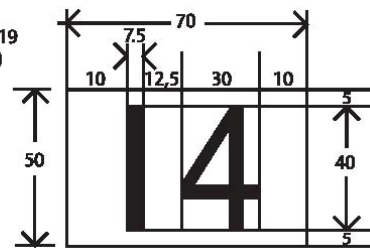
Race numbers must be black in colour, plain and upright and use the Arial font or similar. The number 1 in any number will not be Arial font but a plane upright single line white background must be surrounded by a 2cm thick black line with 12.5cm radius corners.

They shall conform to the following minimum dimensions: Height: 40cm Width: 30cm Thickness: 7.5cm Spacing:10cm (see diagrams). The numbers shall be positioned as far forward as possible. The distance between the Race boat number area and any advertising will be 150mm.

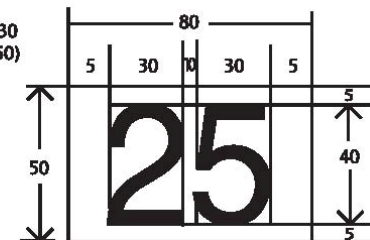
**Diagram numbers 1 to 9  
(White cm 60x50)**



**Diagram numbers 10 to 19  
and 21 (White cm 70x50)**



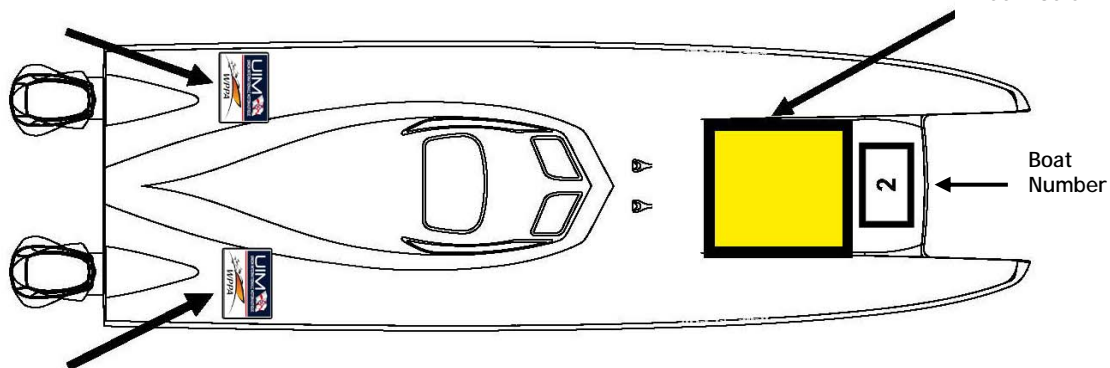
**Diagram numbers 20 to 30  
except 21 (White cm 80x50)**



**TOP VIEW**

UIM & WPPA 40 X 50

LOC SPACE  
100 X 80 cm



UIM & WPPA 40 X 50

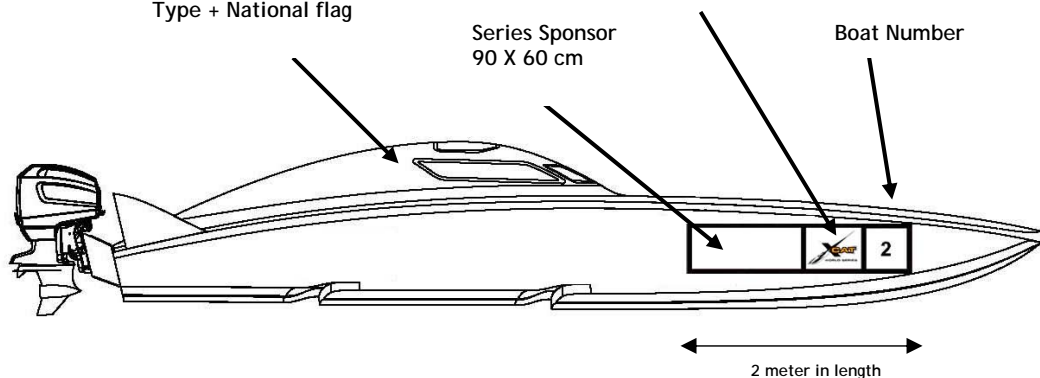
**SIDE VIEW**

Driver's name + Blood  
Type + National flag

XCAT 50 X 50 Both Sides

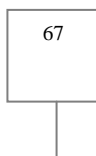
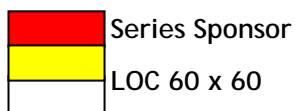
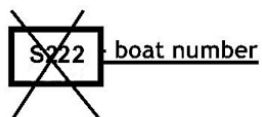
Series Sponsor  
90 X 60 cm

Boat Number

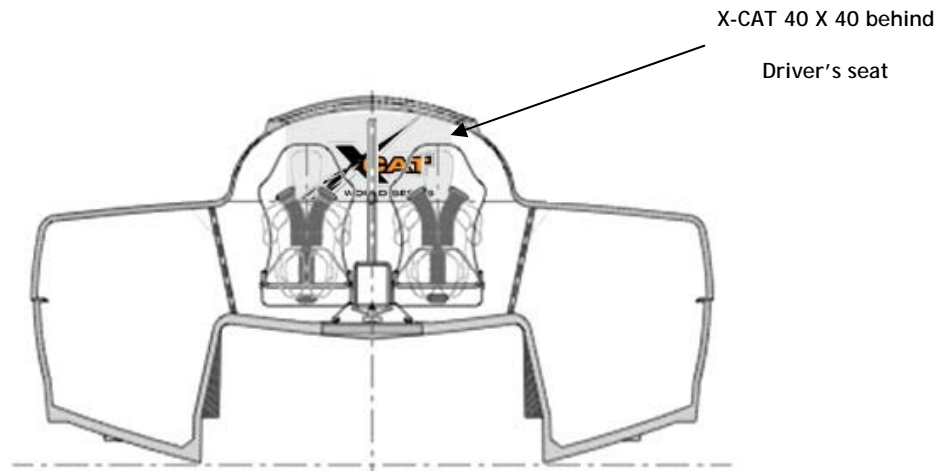


The side boat/hull branding must be the same in both sides

NOTE: Letters like (S) side by side the boat number is prohibited:



## INSIDE THE COCKPIT



### TEAMS/ PILOTS LIABILITY

- Teams/ Competitors are responsible to comply with the Advertising and branding code contained in the 2012 UIM X-CAT Racing Rules and any and all regulations issued by the Series Promoter.
- Teams may submit a diagram of their Branding layout to the Series Promoter for approval.
- Branding shall be kept in a clean and tidy condition and stickers shall be properly applied and maintained. Teams shall maintain the branding material provided by the Series Promoter and the Event Organiser.
- Teams/Competitors attention is drawn to Appendix G- Regulation I-1 & I-2: if the branding does not comply with the Rules:
- the Technical Director or the Race Director may apply a penalty to any non complying boat.
- the UIM & Series Promoter may penalize any non complying boat.

REGULATION 1.2

RACE OVERALL



UIM & WPPA logo should not be less than 05cm each.

X-CAT logo should not be less than 08cm

